


# AGENDA

## **NORTHERN BEACHES COUNCIL LOCAL TRAFFIC COMMITTEE MEETING**

Notice is hereby given that a meeting of the Northern Beaches Council Local Traffic Committee will be held in the Flannel Flower Room, Civic Centre, Dee Why on

**TUESDAY 5 APRIL 2022**

Beginning at 10.00 am for the purpose of considering and determining matters included in this agenda.



**Jorde Frangoples**  
**Director Transport and Assets**

## Voting Members

Chair –Northern Beaches Council - Councillor  
 Member for Pittwater Mr R Stokes MP Representative  
 Member for Davidson Mr J O’Dea MP Representative  
 Member for Wakehurst Mr B Hazzard MP Representative  
 Member for Manly Mr J Griffin MP Representative  
 Transport for NSW  
 Northern Beaches Police Command, Dee Why

Mr Jose Manano-Pires  
 Mr Andrew Johnston  
 Mr Phil Corbett  
 Mr Toby Williams  
 Ms Adele Heasman  
 Mr Peter Carruthers  
 Sergeant Nino Jelovic

## Non Voting Members

Keolis Downer Northern Beaches Bus Operations  
 ComfortDelgro Company (ex Forest Coach Lines)  
 Manly Warringah Cabs Cooperative Society Ltd  
 Cycling Representative

Mr Patrick Wu  
 Mr Robert Bicakcian  
 TBC  
 Vacant

## Officers

Director Transport and Assets  
 Executive Manager - Transport and Civil Infrastructure  
 Manager – Transport Network  
 Traffic Engineering Coordinator  
 Traffic Engineer  
 Traffic Engineer  
 Traffic Engineer  
 Traffic Engineer  
 Traffic Officer  
 Traffic Officer  
 Traffic Officer  
 Traffic Officer  
 Traffic Officer  
 Road Safety Officer  
 Strategic Transport Coordinator  
 Traffic Engineering Trainee  
 Traffic Engineering Intern  
 Manager - Rangers  
 Coordinator - Rangers  
 Specialist Administration Officer

Mr Jorde Frangoples  
 Mr Craig Sawyer  
 Mr Phil Devon  
 Mr James Brocklebank  
 Mr Ricky Kwok  
 Mr Velsamy Sankaran  
 Ms Leila Kazemnezhad  
 Mr Scipio Tam  
 Mr Luke Nickson  
 Mr Brian Duong  
 Mr Ali Samimi Haghighi  
 Mr Yuan Ren  
 Mr Jenzy Ocampo  
 Ms Robynann Dixon  
 Ms Michelle Carter  
 Mr Nicholas Murace  
 Mr Michael Kennedy  
 Mr Paul Crossan  
 Mr Michael Davey  
 Ms Caty Pilley

## Visitors

Nil

## **Agenda for a meeting of the Northern Beaches Council Local Traffic Committee**

**to be held on Tuesday 5 April 2022**

**in the Flannel Flower Room, Civic Centre, Dee Why**

**Commencing at 10.00 am**

### **1.0 APOLOGIES**

### **2.0 CONFIRMATION OF MINUTES OF PREVIOUS MEETINGS AND DECLARATION OF PECUNIARY AND CONFLICT OF INTEREST**

2.1 Minutes of Northern Beaches Council Local Traffic Committee held 1 March 2022

2.2 Declaration of Pecuniary and Conflicts of Interest

### **3.0 REPORTS TO PROCEED TO COUNCIL FOR APPROVAL**

Nil

### **4.0 REPORTS FOR APPROVAL BY COUNCIL DELEGATION .....6**

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4.2 Wimbledon Avenue, North Narrabeen - Installation of No Stopping Restriction..... 10

4.3 Vuko Place, Warriewood- Installation of 10P parking restriction..... 14

4.4 Yulong Avenue, Terrey Hills - Installation of No Stopping Yellow Line ..... 18

4.5 Abbott Road, North Curl Curl - Shared Path including Raised Pedestrian Crossings.....22

4.6 Aumuna Road – Larool Road, Terrey Hills – ‘Give Way’ control .....74

4.7 Stuart Street, Manly - No Parking and No Stopping .....77

4.8 Upper Clifford Avenue, Fairlight - No Stopping Unbroken Yellow Kerb Line.....81

4.9 Roseberry Street, Balgowlah - Shared Path, Raised Pedestrian Crossing and Refuge Island .....85

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<b>5.0</b>	<b>MATTERS FOR NOTATION .....</b>	<b>201</b>
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**NEXT MEETING Tuesday 3 May 2022**

## 2.0 CONFIRMATION OF MINUTES OF PREVIOUS MEETINGS AND DECLARATION OF PECUNIARY AND CONFLICTS OF INTEREST

### 2.1 MINUTES OF NORTHERN BEACHES COUNCIL LOCAL TRAFFIC COMMITTEE HELD 1 MARCH 2022

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#### RECOMMENDATION

That the minutes of the Northern Beaches Council Local Traffic Committee held 1 March 2022, copies of which were previously circulated to all members, be confirmed as a true and correct record of the proceedings of that meeting.

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### 2.2 DECLARATION OF PECUNIARY AND CONFLICTS OF INTEREST

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Members should disclose any "**pecuniary**" or "**non-pecuniary**" conflicts of interests in matters included in the agenda. The [Northern Beaches Council Code of Conduct](#) (the Code) provides guidance on managing conflicts of interests.

A **pecuniary interest** is defined in Section 4 of the Code as:

*A pecuniary interest is an interest that you have in a matter because of a reasonable likelihood or expectation of appreciable financial gain or loss to you or a person referred to in clause 4.3.*

A **non-pecuniary conflict of interest** is defined in Section 5 of the Code as:

*A non-pecuniary conflict of interest exists where a reasonable and informed person would perceive that you could be influenced by a private interest when carrying out your official functions in relation to a matter.*

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## 4.0 REPORTS FOR APPROVAL BY COUNCIL DELEGATION

<b>ITEM 4.1</b>	<b>MAHOGANY BOULEVARD- INSTALLATION OF NO STOPPING YELLOW LINES.</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2021/735301</b>
<b>ATTACHMENTS</b>	<b>1 Plan</b> <b>2 Table of Consultation</b>

**GEOCODES: -33.691139, 151.288528**

### REPORT

#### BACKGROUND

Council has received concerns from local residents regarding parked vehicles on Mahogany Boulevard close to the existing pedestrian island that obstruct views for the pedestrians who intend to cross the road safely, due to the narrow width of the road. Furthermore, parked vehicles at the abovementioned location impede side views for the exiting vehicles from the driveways into Mahogany Boulevard safely.

#### LOCATION

Mahogany Boulevard is a local road of 4.5m in width, low volumes of two-way traffic. Mahogany Boulevard intersects Garden Street on its eastern side and it meets Acacia Circuit on its south western side. No Bus route services Mahogany Boulevard. No Parking restriction is allocated on Mahogany Boulevard.

#### ISSUES

- Parked vehicles on Mahogany Boulevard close to the existing pedestrian island obstruct views for the pedestrians who intend to cross the road safely.
- Parked vehicles at the abovementioned location impede side views for the exiting vehicles from the driveways into Mahogany Boulevard safely.

#### PROPOSAL

Council has undertaken a review of the location and issues and proposes the installation of 20m length of unbroken No Stopping yellow lines on both sides of Mahogany Boulevard, between Garden Street and the eastern sides of driveways No's 1 and 2 Mahogany Boulevard, in addition to the installation of a 10m statutory No Stopping yellow line at eastern side of the intersection at Acacia Circuit and Mahogany Boulevard.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have impact on people cycling and does not affect any future planned facilities
- The proposal does affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

Consultation letters have been distributed to 7 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

- A total of 3 submissions were received with no objections and 3 supporting the proposal.




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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the:

- A. The installation of 20m length of unbroken No Stopping yellow lines on both sides of Mahogany Boulevard between Garden Street and the eastern sides of driveways No's 1 and 2 Mahogany Boulevard.
- B. The installation of a 10m statutory No Stopping yellow line at eastern side of the intersection at Acacia Circuit and Mahogany Boulevard.



	PROPOSAL		 northern beaches council
	Mahogany Boulevard, WARRIEWOOD Installation of No Stopping Yellow Lines		
	Drawn: A.S.H	Approved 	

<b><u>Table of Consultation</u></b>		
<b>Address</b>	<b>Mahogany Boulevard, Warriewood</b>	
<b>Proposal</b>	<b>Installation of No Stopping Yellow Lines</b>	
<b>Properties Consulted</b>	<b>7</b>	
<b>Responses Received</b>	<b>3</b>	
<b>Support</b>	<b>3</b>	
<b>Do Not Support</b>	<b>0</b>	
<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
<b>Road Safety</b>	When residents/trades (daily) park very close to where pedestrians cross the road at Mahogany Boulevard, their view is extremely impeded due to the parked cars, sadly it is only a matter of time where someone is going to get hit by a car.	Council's proposal will resolve this issue by providing clear spaces around the existing pedestrians island that minimise the amount of risks and hazards for the passage of pedestrian at the proposed location.

<b>ITEM 4.2</b>	<b>WIMBLEDON AVENUE, NORTH NARRABEEN - INSTALLATION OF NO STOPPING RESTRICTION</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2021/792446</b>
<b>ATTACHMENTS</b>	<b>1 Plan 2 Table of Consultation</b>

**GEOCODES: -33.711159, 151.292114**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding vehicles parking on Wimbledon Avenue close to its intersection with Wakehurst Parkway. This causes hazardous conditions for vehicles seeking to enter Wimbledon Avenue as the parked vehicles leave insufficient road width for an entering vehicle to pass another vehicle trying to exit the street. Parked vehicles also impede sight lines to pedestrians and cyclists crossing Wimbledon Avenue on the Narrabeen Lagoon Shared path.

### **LOCATION**

Wimbledon Avenue is a local road of 7.8m in width, carrying low volumes of two-way traffic. It is a loop road meeting Wakehurst Parkway at two points. It is a purely residential street, although there is a reserve with a frontage to Narrabeen Lagoon at its southern end, which attracts picnickers and recreational fishermen. The western leg of Wimbledon Avenue has the statutory 10m length of No Stopping restriction on its eastern side but has no parking restrictions on its western side. No bus route travels along Wimbledon Avenue.

### **ISSUES**

- Parked vehicles on the western leg of Wimbledon Avenue close to its intersection with the Wakehurst Parkway, cause hazardous conditions for vehicles seeking to enter Wimbledon Avenue. The narrow width of the street leaves insufficient width for entering and exiting vehicles to pass each other.
- Parked vehicles impede sight lines to pedestrians and cyclists crossing Wimbledon Avenue on the Narrabeen Lagoon Shared path.

### **PROPOSAL**

Council has undertaken a review of the location and issues and proposes to extend the existing No Stopping zone on the eastern side of Wimbledon Avenue by 10m, and to install a 10m length of No Stopping zone on the western side of Wimbledon Avenue, between driveway No.107 and Wakehurst Parkway.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have impact on people cycling and does not affect any future planned facilities
- The proposal does affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

Consultation letters have been distributed to 13 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

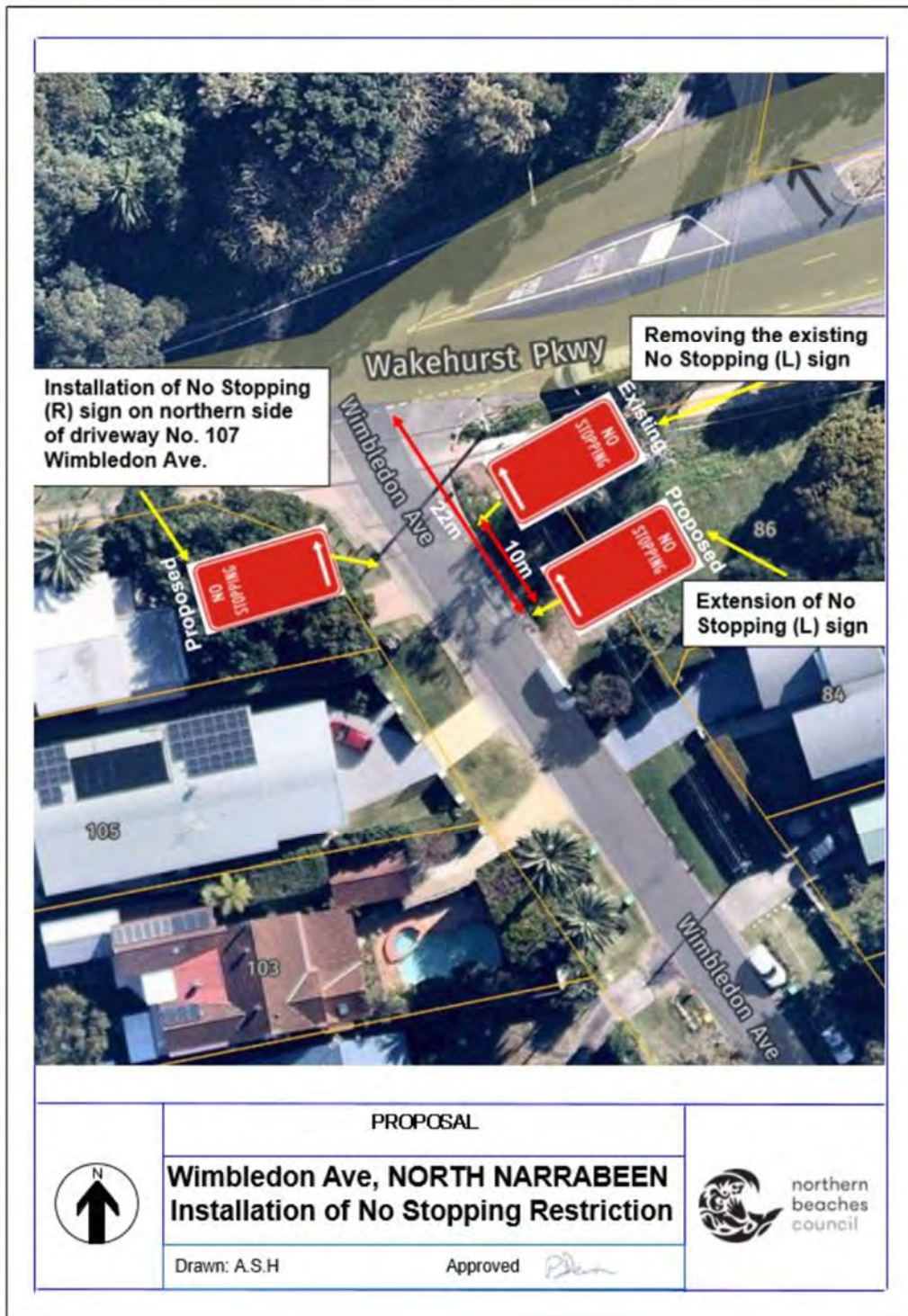
- A total of 1 submission was received with no objections and 1 supporting the proposal.

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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the:

- A. Extension of the existing No Stopping zone on the eastern side of Wimbledon Avenue by an additional 10m.
- B. Installation of a No Stopping zone on the western side of Wimbledon Avenue between the driveway to No.107 Wimbledon Avenue and the Wakehurst Parkway.



<u><b>Table of Consultation</b></u>		
<b>Address</b>	<b><u>Wimbledon Avenue, NORTH NARRABEEN</u></b>	
<b>Proposal</b>	<b><u>Installation of No Stopping Restriction</u></b>	
<b>Properties Consulted</b>	<b>13</b>	
<b>Responses Received</b>	<b>1</b>	
<b>Support</b>	<b>1</b>	
<b>Do Not Support</b>	<b>0</b>	
<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
<b>Road Safety</b>	The parking situation in this street has been an ongoing issue separating neighbours. The street becomes congested with cars as more and more people want to access the Narrabeen Lake walk. This makes things very dangerous.	Council's proposal will create clear travel lanes on both sides of Wimbledon Avenue near its intersection with Wakehurst Parkway and will improve safety issues for the passage of various vehicles.

**ITEM 4.3**                      **VUKO PLACE, WARRIEWOOD- INSTALLATION OF 10P  
PARKING RESTRICTION****REPORTING OFFICER**        **TRAFFIC OFFICER****TRIM FILE REF**              **2022/034953****ATTACHMENTS**            **1 Table of Consultation**  
                                      **2 Plan****GEOCODES: -33.693192, 151.302795****REPORT****BACKGROUND**

Council has received concerns from local residents regarding parked boats and trailers in Vuko Place that reduce parking availability for workers and customers in the Industrial Area and can impede sight distance for vehicles exiting from driveways.

**LOCATION**

Vuko Place is a local road of 11.5m in width, carrying medium volumes of two-way traffic. Vuko Place intersects Warriewood Road on its northern side, and it ends to a cul-de-sac on its southern side. There is a No Stopping zone between the driveway No.2 and the driveway No.3 Vuko Place. There is no bus route that services Vuko Place.

**ISSUES**

- Parked boats and trailers on Vuko Place reduce parking availability for workers and customers in the Industrial Area and can impede sight distance for vehicles exiting from driveways.

**PROPOSAL**

Council has undertaken a review of the location and issues and proposes the installation of 10P time parking restrictions, (7am to 7pm, EVERYDAY) along both sides of Vuko Place.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will not impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

Consultation letters have been distributed to 89 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

- A total of 8 submissions were received with 2 objections and 6 supporting the proposal.

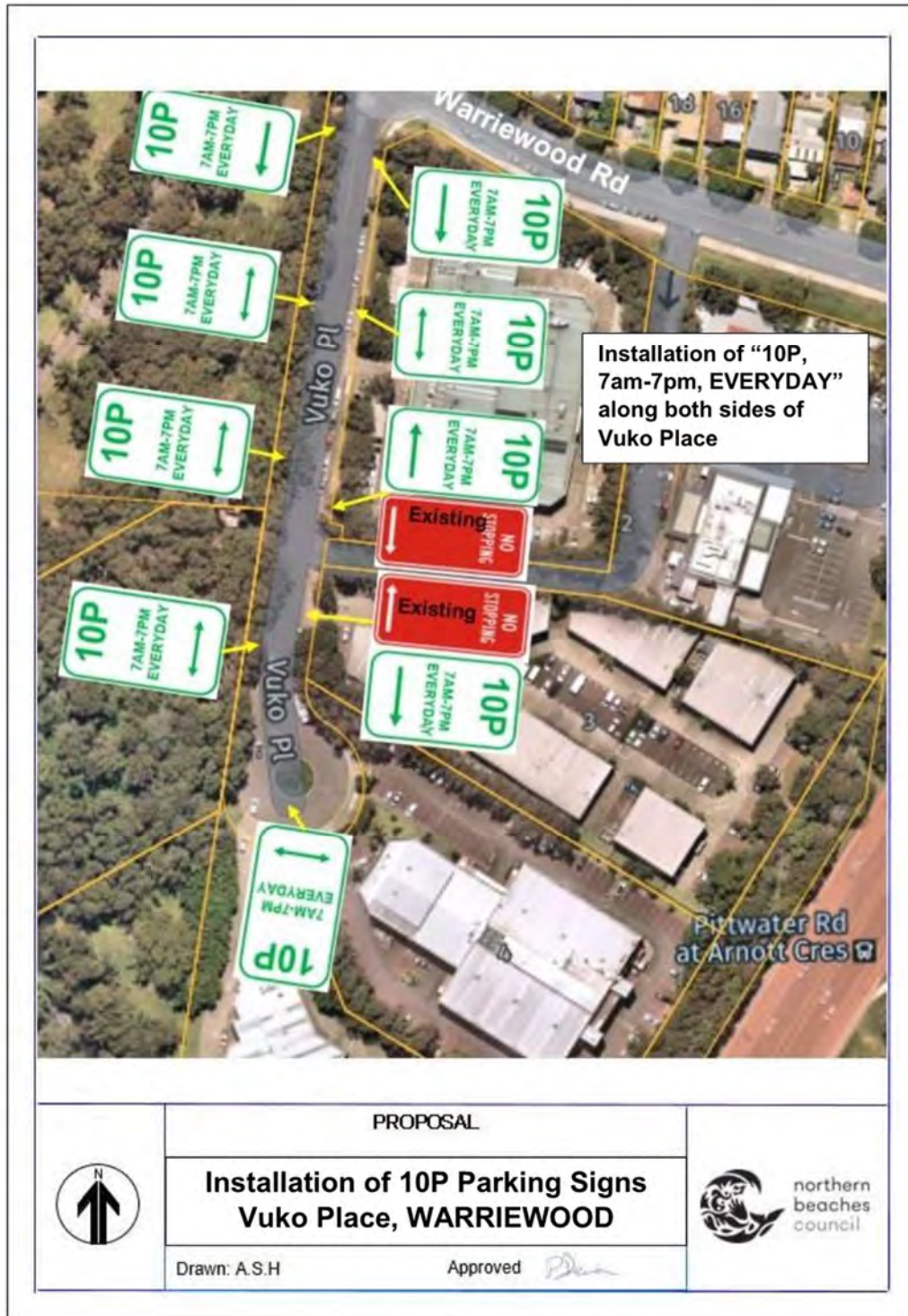
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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports:

- A. The installation of 10P time parking restrictions, (7am to 7pm, EVERYDAY) along both sides of Vuko Place.

<u><b>Table of Consultation</b></u>		
<b>Address</b>	<u><b>Vuko Place, WARRIEWOOD</b></u>	
<b>Proposal</b>	<u><b>Installation of 10P Parking Restriction</b></u>	
<b>Properties Consulted</b>	<b>89</b>	
<b>Responses Received</b>	<b>8</b>	
<b>Support</b>	<b>6</b>	
<b>Do Not Support</b>	<b>2</b>	
<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
<b>Parking Demand</b>	<b>I REQUIRE INTERMITTENT 24 HOUR PARKING FOR MY FLEET OF VEHICLES. As professionals I assume that the traffic management division of council can explain where these legally registered vehicles will be parked if it is not in these industrial areas.</b>	<b>Long term parking of private vehicles, boats and trailers must be on private properties as the residents are responsible to utilise garages and existing driveways for off-street parking in case of increased parking demands.</b>
<b>Lack of Parking</b>	<b>We cannot park in Vuko itself due to the amount of trailers, trucks, and boats that people just park and leave there. Not only this, due to the size of these vehicles and how closely they park to the driveways there have been a number of near misses with traffic incidents simply as we cannot see around them.</b>	<b>Council proposes the installation of 10P time parking restrictions, (7am to 7pm, EVERYDAY) in order to resolve the issue by providing more available car spaces for the business holders and the vistor's of the area.</b>



<b>ITEM 4.4</b>	<b>YULONG AVENUE, TERREY HILLS - INSTALLATION OF NO STOPPING YELLOW LINE</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2021/789517</b>
<b>ATTACHMENTS</b>	<b>1 Table of Consultation</b> <b>2 Plan</b>

**GEOCODES: -33.682320, 151.229477**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding parked vehicles, caravans and trailers on the south side of Yulong Avenue along the frontage of No.9A Yulong Avenue that cause insufficient sight distance and unsafe conditions for vehicles exiting driveways. Furthermore, parked vehicles at the abovementioned location restrict the road width and create potentially unsafe conditions for opposing traffic meeting at the bend.

### **LOCATION**

Yulong Avenue is a local road of 9m in width, carrying medium volumes of two-way traffic. This road meets McCarrs Creek Road on its eastern side and meets Booralie Road on its western side. The proposed location has been located on southern side of the exiting bend on Yulong Avenue. There is a No Stopping restriction on the opposite side of the proposed location. There is a bus route that services Yulong Avenue.

### **ISSUES**

- Parked vehicles, caravans and trailers on the southern side of Yulong Avenue along the frontage of No.9A Yulong Avenue, cause insufficient sight distance and unsafe conditions for vehicles exiting driveways.
- Parked vehicles at the abovementioned location restrict the road width and create potentially unsafe conditions for opposing traffic meeting at the bend.

### **PROPOSAL**

Council has undertaken a review of the location and issues and proposes the installation of a 25m length of No Stopping yellow line on the southern side of Yulong Avenue, between the western side of driveway No.9A and the eastern side of driveway No.9B.

### **PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

## CONSULTATION

Consultation letters have been distributed to 12 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

- No submissions were received with no objections and none supporting the proposal.

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## RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee supports the:

- A. Installation of a 25m length of No Stopping yellow line on southern side of Yulong Avenue, between the western side of driveway No.9A and the eastern side of driveway No.9B.

<u>Table of Consultation</u>		
Address	<u>Yulong Avenue, TERREY HILLS</u>	
Proposal	<u>Installation of Unbroken No Stopping Yellow Line</u>	
Properties Consulted	12	
Responses Received	0	
Support	0	
Do Not Support	0	
Issue	Resident Comment	Council Response
Lack of side view	Cars are parked on both sides of the road making it dangerous as well as difficult to get in and out our driveway. Buses, cyclists and people park caravans and trailers there for extended periods. We live on the curve and it is extremely dangerous as we can't see oncoming traffic.	Council proposes the installation of No Stopping yellow line on inner side of the existing bend on Yulong Avenue in order to improve the side views for the exiting vehicles from the driveways.



<b>ITEM 4.5</b>	<b>ABBOTT ROAD, NORTH CURL CURL - SHARED PATH INCLUDING RAISED PEDESTRIAN CROSSINGS</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/061841</b>
<b>ATTACHMENTS</b>	<b>1 Abbott Road, North Curl Curl - Shared Path Design 2 Abbott Road, North Curl Curl - Shared Path - Community Engagement Report</b>

**GEOCODES: -33.765800, 151.289625**

## **REPORT**

### **BACKGROUND**

Council has successfully obtained funding through the Federal Stimulus - School Infrastructure Program to develop a safer pedestrian and bicycle route along Abbott Road, North Curl Curl.

The proposed 2.0m to 3m wide concrete shared path will provide a safer pedestrian and bicycle route connection to Northern Beaches Secondary College and Curl Curl North Public Schools along the Abbott Road corridor.

The proposal will provide a desirable walking and cycling option, encouraging active transport and hopefully reducing general traffic in the area longer term. The work is in line with our Move Northern Beaches Transport Strategy and the Northern Beaches Bike Plan.

### **LOCATION**

- Abbott Road is both a Regional Road (between Harbord Rd to Pitt Road – posted speed limit of 50km/h) and Local Road (between Pitt Road to Griffin Road – posted speed limit of 40km/h).
- Northern Beaches Secondary College - Manly Selective Campus and Curl Curl North Public School are located on the northern side of Abbott Road along with residential properties.
- Sporting ovals, courts and facilities (including Youth and Community Centre) are located along the whole southern side of Abbott Road.

### **ISSUES**

The feedback collected during consultation indicated a high level of support (over 70 percent) for the proposed shared path upgrade, citing the anticipated benefits for all path users. However, many comments stated a preference for the path to be located on the southern side of Abbott Road adjacent to the netball courts and sporting fields to avoid conflicts with resident driveways. Council has revised the concept plan based on this feedback.

The proposed path will now be located on the southern side of Abbott Road between Pitt Road and Playfair Road. This location is now possible due to the coinciding works to the netball courts.

## PROPOSAL

Council has undertaken a review of the location and issues and proposes to install a 2.1m to 3m wide concrete shared path along Abbott Road, North Curl Curl.

The proposed path is to commence along the southern side from opposite Northern Beaches Secondary College - Manly Selective Campus to the existing raised pedestrian crossing (proposal to install a cycleway priority crossing at this existing raised pedestrian crossing) at Curl Curl North Public School.

The existing footpath along the northern side at Curl Curl North Public School is to be widened, to then cross back to the southern side at the proposed raised pedestrian and cycleway priority crossing at the existing traffic islands outside 60 Abbott Road.

The proposed shared path is then to continue along the southern side of Abbott Road until the existing refuge crossing at Griffin Road, North Curl Curl.

## PEDESTRIAN AND CYCLING IMPACT STATEMENT

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have a positive impact on pedestrians and bicycle riders by providing a wider path with safer road crossings for pedestrians and provide the option of an off-road route for bicycle riders.

## CONSULTATION

The project was open for community engagement from 23 August to 19 September 2021. Consultation letters were distributed to 89 property owners and occupiers within the immediate vicinity of the location and 12 signs were placed along the proposed project route.

An onsite meeting was held with 'Friends of Curl Curl Lagoon' and direct emails sent out regarding the project to 14 key stakeholders (including nearby Schools, Community and Sporting clubs).

In addition, the project was listed on Council's Your Say website and advertised through Council's community engagement channels. Council received 135 submissions, 70% were in support of the project, 25% non-support and 5% did not show preference. The responses are noted in Attachment 2 – Community Engagement Report.

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## RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee supports the:

- A. Installation of a 2.0m to 3m wide concrete shared path along Abbott Road, North Curl Curl.
- B. Installation of a cycleway priority crossing at the existing raised pedestrian crossing outside Curl Curl North Public School.
- C. Installation of a raised pedestrian and cycleway priority crossing at the existing traffic islands outside 60 Abbott Road.



### LEGEND

	NEW STANDARD CONCRETE FOOTPATH 100mm THICK 25MPa COMPRESSIVE STRENGTH, SLT2 MESH REINFORCED PLAIN CONCRETE BROOM FINISH		NEW PEDESTRIAN ACCESS RAMPS 25MPa COMPRESSIVE STRENGTH MIX COVE FINISH (REFER TO DETAIL)
	NEW CONCRETE PIAM RAMP 25MPa COMPRESSIVE STRENGTH MIX MIN 500 HEAL COVE FINISH		NEW CONCRETE KERB AND GUTTER 150 HIGH (25MPa STRENGTH)
	RAISED FIBRE REINFORCED PANEL		

DRAWING INDEX


DRAWING NO.	DESCRIPTION	ISSUE	REVISION
0001	COVER SHEET	CONCEPT	1
1001-1008	PLANS 1-8	CONCEPT	1
2001-2002	LONG SECTIONS 1-2	CONCEPT	1
3001-3003	CROSS-SECTIONS 1-3	CONCEPT	1
4001-4004	DETAILS 1-4	CONCEPT	1

CAUTION – SERVICES!!!



- THE POSITIONS OF SERVICES SHOWN ON THE DRAWING ARE INDICATIVE ONLY AND HAVE BEEN PLOTTED FROM PLANS SUPPLIED BY THE RELEVANT UTILITY AUTHORITIES.
- PITS, POLES, MARKER POSTS, SIGNS, ETC. HAVE BEEN PLOTTED ON THE DRAWINGS WHERE SIGHTED AT THE TIME OF SURVEY BUT THE SURVEY DOES NOT INCLUDE DETAILED INVESTIGATION OR PHYSICAL LOCATION OF UNDERGROUND INFRASTRUCTURE.
- THE RESPONSIBILITY FOR OBTAINING CONTRACTS PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ACTIVITY ON OR ADJACENT TO THE SITE TO OBTAIN UPDATED SERVICES DIAGRAMS THROUGH A DIAL-BEFORE-YOU-DIG SEARCH AND PHYSICAL SEARCH TO ESTABLISH AND CONFIRM THE EXACT LOCATION/S AND DEPTH/S OF ALL UNDERGROUND SERVICES, PRIOR TO COMMENCEMENT,

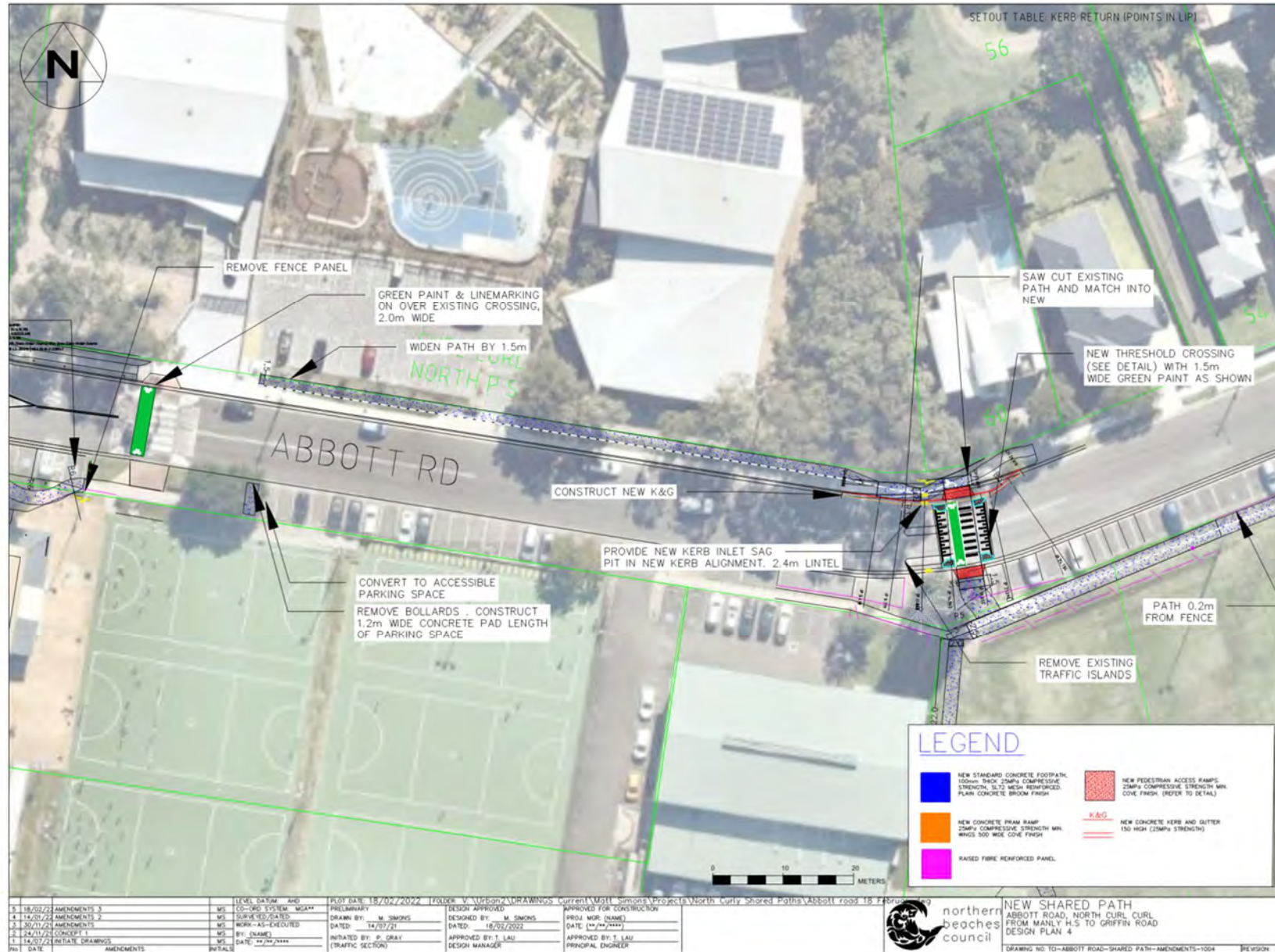
CAUTION: LANDFILL GAS MONITORING WELLS PRESENT ALONG SITE OF WORKS, PLOTTED AS PINK CIRCLES. SEEK ADVICE FROM COUNCIL PARKS FOR ADVICE REGARDING ADJUSTMENT OF WELLS.

13/10/22		AMENDMENTS 3	MS	CO-ORD SYSTEM: MGA84	LEG: DATUM: AHD		PROJECT DATE: 18/02/2022		PROJECT: Y Urban/URAWINGS Current/Mail/Storm/Projects/North Curl/Shared Paths/Abbott road 18 #Broomberg		 northern beaches council	NEW SHARED PATH ABBOTT ROAD, NORTH CURL FROM MANLY H.S TO GRIFFIN ROAD COVER SHEET
14/10/22		AMENDMENTS 2	MS	SURVEY DATED	DRAIN BY: M. SIMONS		DESIGNED BY: M. SIMONS		PROJ WDR: (NAME)			
14/10/22		AMENDMENTS	MS	WORK-AS-EXECUTED	DATE: 14/07/21		DATE: 30/11/21		DATE: 29/09/2022			
20/10/22		CONCEPT 1	MS	BY: (NAME)	INITIATED BY: P. GRAY		APPROVED BY: T. LAU		APPROVED BY: T. LAU			
20/10/22		FINAL DRAINAGE DRAWINGS	MS	DATE: 22/02/2022	DATE: 22/02/2022		DATE: 22/02/2022		DATE: 22/02/2022			
No DATE		AMENDMENTS	INITIALS									DRAWING NO: NORTH CURL-SHARED PATH-AMENDMENTS-0001 REVISION:







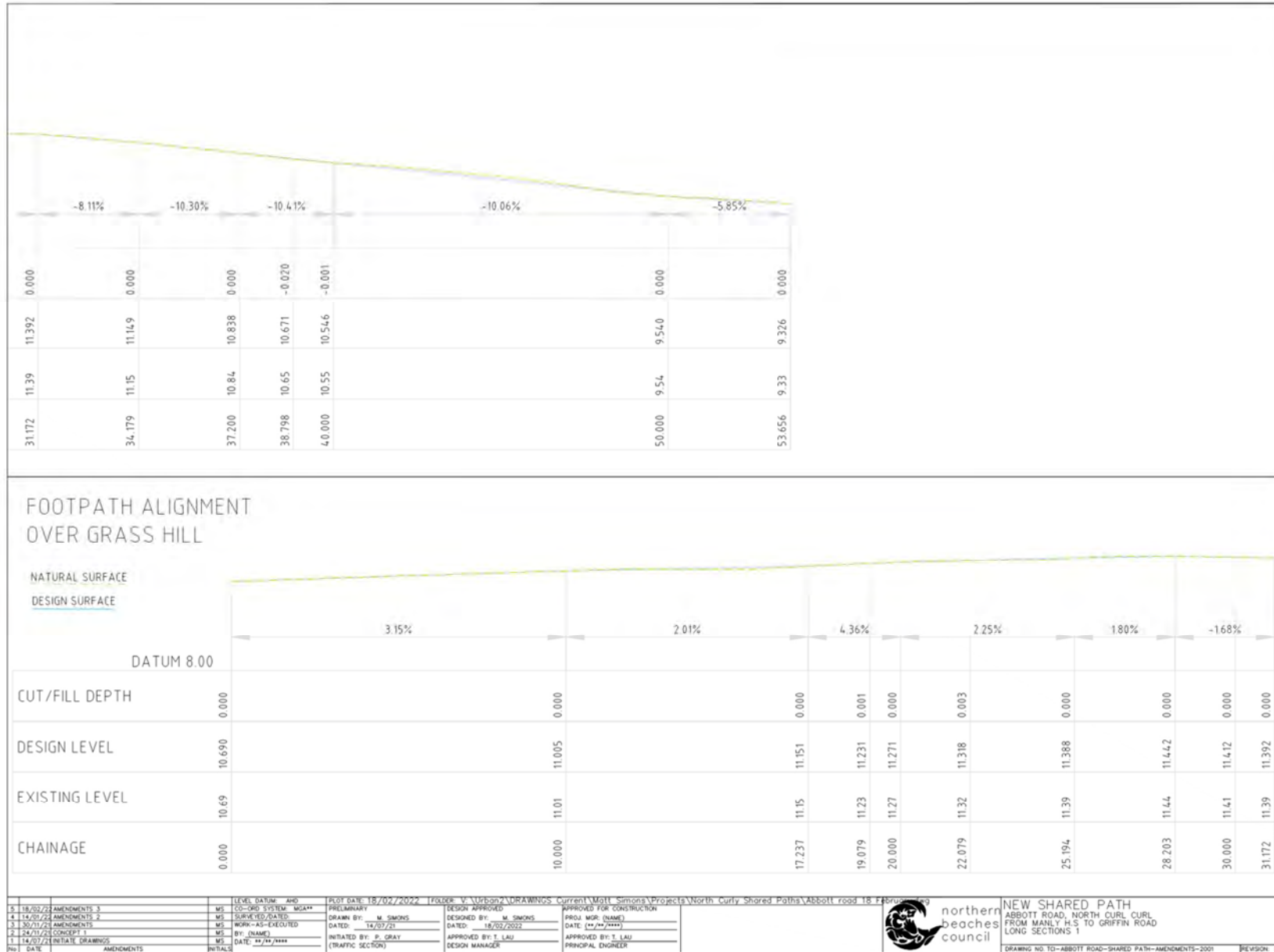








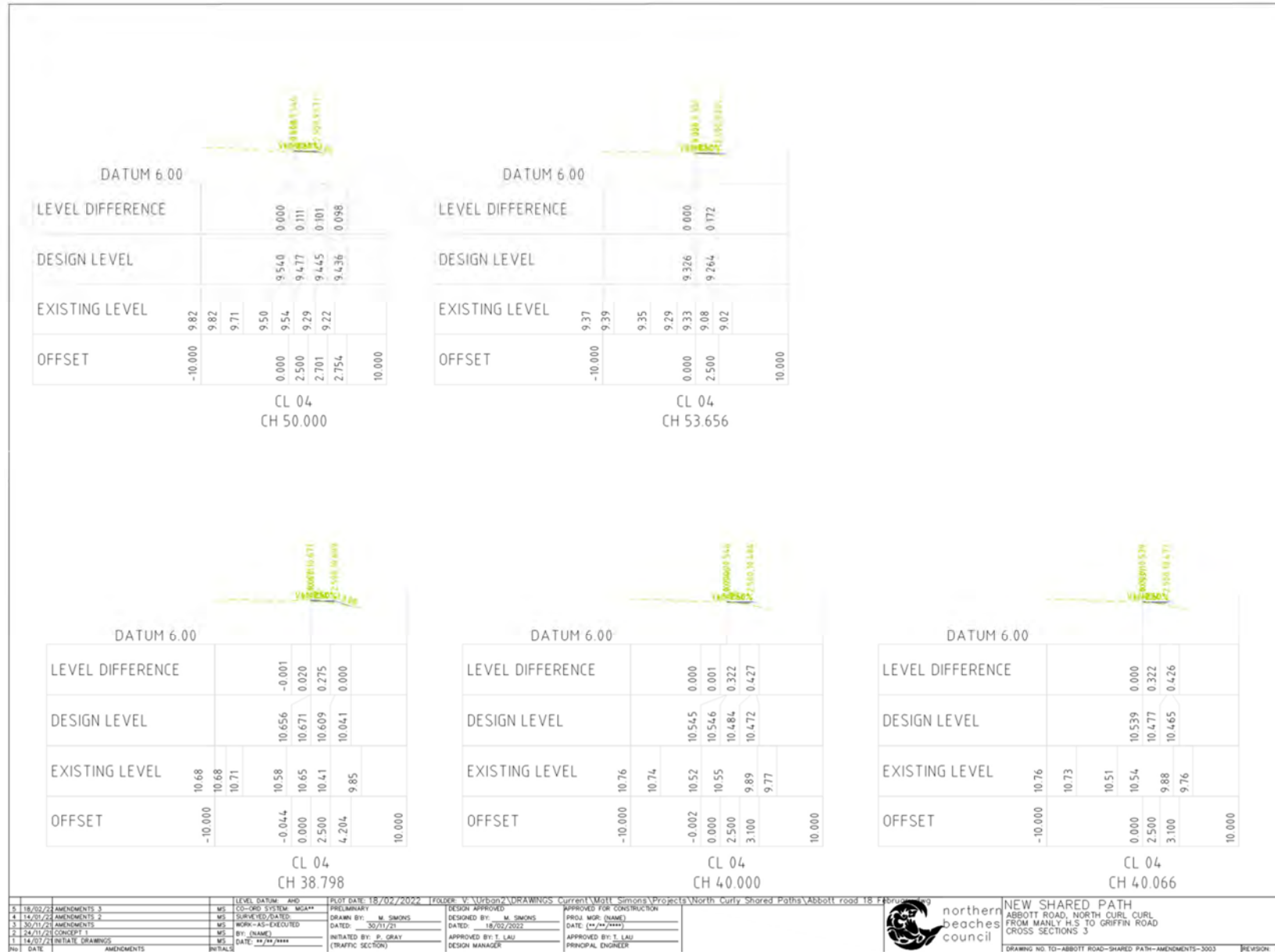


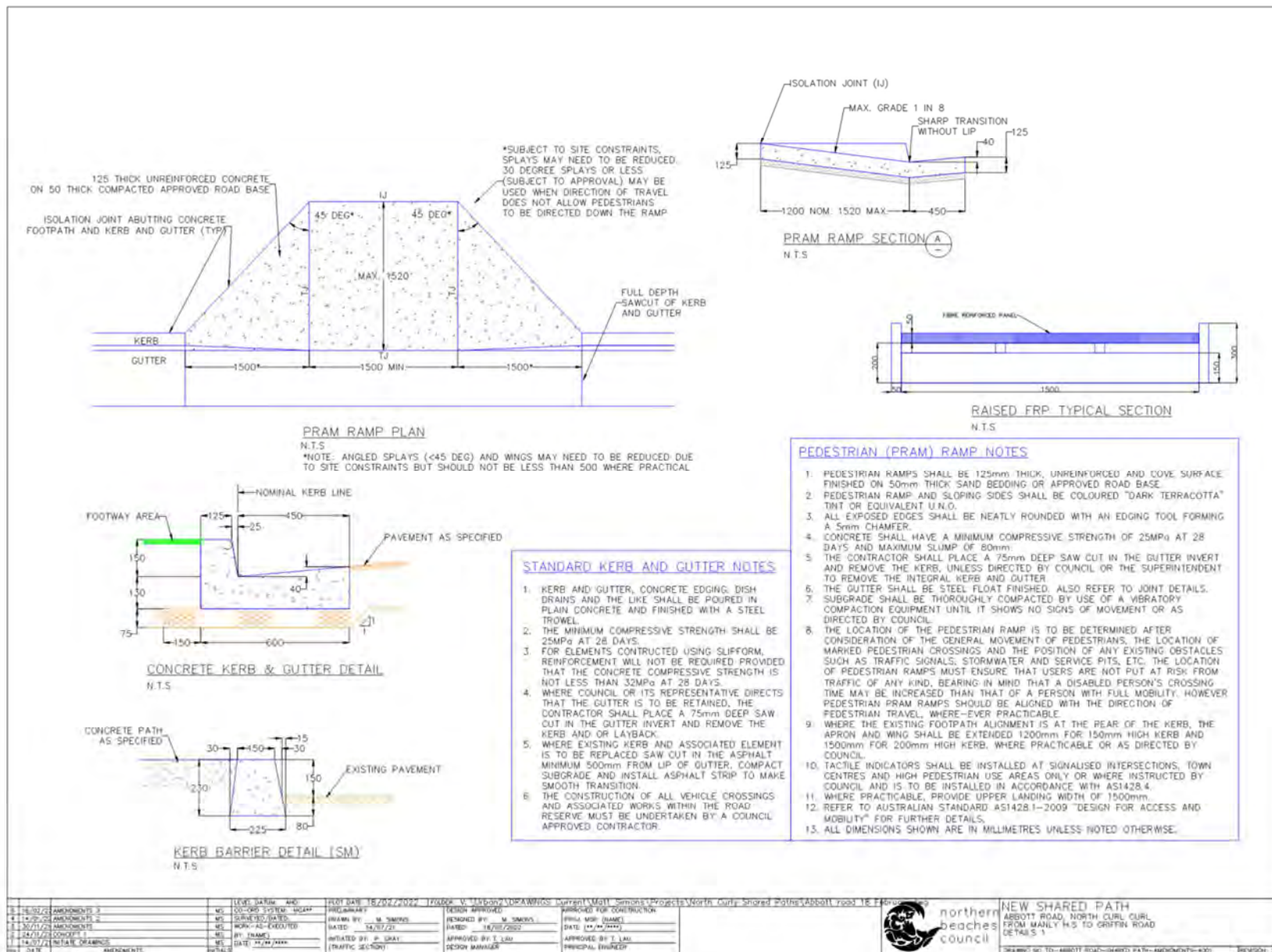






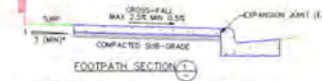




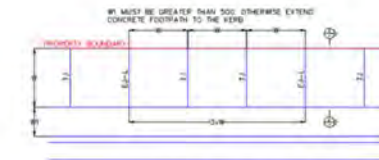




FOOTPATH PLAN - KERB SIDE  
N.T.S.



FOOTPATH SECTION 1  
N.T.S.



FOOTPATH PLAN - BOUNDARY SIDE  
N.T.S.



FOOTPATH SECTION 2  
N.T.S.

**IMPORTANT NOTE**  
THESE STANDARDS WERE PRODUCED FOR THE SOLE USE OF THE NORTHERN BEACHES COUNCIL. UNLESS THE STANDARD DETAILS ARE INDICATED ON CONSTRUCTION DRAWINGS WHICH HAVE BEEN APPROVED BY NORTHERN BEACHES COUNCIL, THEY ARE NOT TO BE USED FOR ANY OTHER PURPOSE WITHOUT THE WRITTEN PERMISSION OF NORTHERN BEACHES COUNCIL'S ASSET MANAGER.

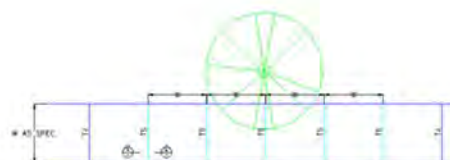
DRAWINGS COLOUR CODES  
PLEASE PRINT ALL COPIES IN COLOUR



FOOTPATH PLAN - CENTRED  
N.T.S.



FOOTPATH SECTION 3  
N.T.S.

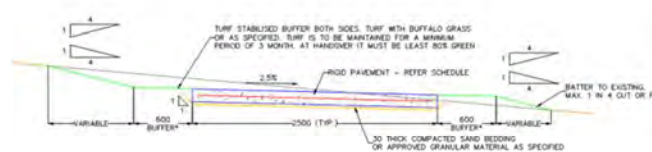


FOOTPATH PLAN NEAR TREE PLAN  
N.T.S.



FOOTPATH SECTION 4  
N.T.S.

TRANSVERSE JOINTING SYSTEM NEAR TREE ROOTS (TS) SECTION 4  
N.T.S.



SHARED PEDESTRIAN AND BICYCLE PATH IN RIGID PAVEMENT (CONCRETE) DETAIL  
N.T.S.

• WHERE SUFFICIENT WIDTH ALLOWS

FOOTPATH WIDTH (mm)	SLAB THICKNESS (mm)	DISTANCE BETWEEN TRANSVERSE JOINTS (mm)	DISTANCE BETWEEN EXPANSION JOINTS (mm)	REINFORCEMENT (mm)
2100	100	2100	6500	16/72
2500	100	2500	7500	16/72

**STANDARD CONCRETE FOOTPATH NOTES**

- FOOTPATHS TO HAVE A MAX. 2.5% CROSSFALL TOWARDS THE KERB (APPROXIMATELY 37.5mm FALL OVER A 1.5m WIDE FOOTPATH) AND BROAD FINISHED (A.D.)
- CONCRETE EDGES SHALL BE FINISHED WITH AN EDGING TOOL.
- CONCRETE SHALL HAVE A 28 DAY COMPRESSIVE STRENGTH OF 25MPa MINIMUM UNLESS OTHERWISE SPECIFIED.
- CONCRETE SHALL BE PLACED WITH A MAXIMUM SLUMP OF 75mm.
- MINIMUM CONCRETE COVER (TO REINFORCEMENT) TO BE 30mm UNLESS NOTED OTHERWISE.
- CONCRETE FOOTPATHS SHALL BE Laid ON A MINIMUM 75mm THICK ROAD BASE (DOR) (COMPACTED TO MINIMUM SITE MAXIMUM DRY DENSITY) OR 50mm THICK SAND (WELL COMPACTED TO DENSITY INDEX OF NOT LESS THAN 85).
- COUNCIL REQUIRES 24 HOURS NOTICE PRIOR TO POURING OF CONCRETE TO INSPECT THE FORMWORK. NO CONCRETE SHALL BE POURED UNTIL THE EXCAVATION AND FORMWORK HAVE BEEN INSPECTED.
- EXCAVATE TO MINIMUM UNIFORM CONCRETE SLAB THICKNESS AND REINFORCEMENT AS SPECIFIED REFER TO DETAILS.
- PLAIN CONCRETE IS TO BE USED EXCEPT FOR PEDESTRIAN RAMPS (GRASS RAMPS) WHICH WILL BE COLOURED TYPING TERRAZZO/STONE OR EQUIVALENT.
- WHERE THE SLAB IS TO BE POURED ONTO EXISTING ROCK OR ONTO A CONCRETE SUBGRADE, PROVIDE A COAT OF PRIME BOND BREAKER BETWEEN THE INTERFACES TO ENSURE THAT THE CONCRETE WILL SET EVENLY THROUGHOUT THE WHOLE SECTION OF THE SLAB (EVEN SHIMMAGE CONTROL).
- PLACE REINFORCEMENT FABRIC CENTRALLY USING SEATS AS PROPS AND ENSURING THAT THERE WILL BE AT LEAST 30mm MINIMUM COVER (FOR FOOTPATH SLABS) BETWEEN THE REINFORCEMENT AND EXTERNAL SURFACE OF THE SLAB.
- CONCRETE IS TO BE FULLY CURED TO ENSURE THAT IT DOES NOT RESULT IN SHIMMAGE CRACKS. HIGHER STRENGTH CONCRETES TEND TO SET QUICKER AND REQUIRES PROPER CURING BY KEEPING IT CONTINUOUSLY WET FOR A MINIMUM OF 7 DAYS IMMEDIATELY AFTER THE POUR OR BY COVERING WITH CLEAR PLASTIC SHEETS.
- ALL CONCRETE WORKS SHALL BE IN ACCORDANCE WITH AS 3600.
- COMPRESSIBLE FILLER BOARD USED AS CONSTRUCTION JOINTS SHALL BE BITUMEN IMPREGNATED FIBREGLASS.
- SLAB JOINTS WHERE REQUIRED ARE TO BE CUT AFTER THE CONCRETE HAS SUFFICIENTLY HARDENED THAT IT WILL NOT BE DAMAGED BY THE SAWING BUT BEFORE SHIMMAGE CRACKS CAN OCCUR.
- PROVIDE "SMART URBAN" OR "LOCK SOCKETS" AS SPECIFIED FOR ALL SIGN POSTS U.N.C.
- ALL DIMENSIONS SHOWN ARE IN MILLIMETRES UNLESS NOTED OTHERWISE. DIMENSIONS SHALL TAKE PRECEDENCE OVER SCALES.

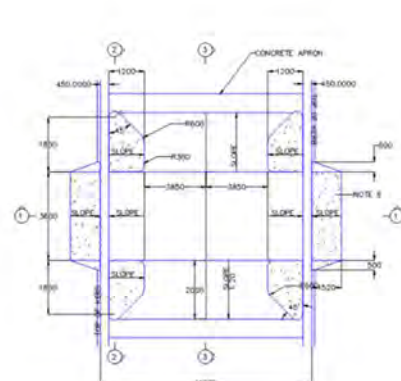
**TRANSVERSE DISPLACEMENT JOINT NOTES**

- WHERE THERE IS LIKELY TO BE TRANSVERSE OR VERTICAL MOVEMENT OF JOINTS IN THE RIGID PAVEMENT (FOR EXAMPLE NEAR A TREE, WHERE DIVURGENT ROOTS ARE LIKELY TO DISPLACE THE PAVEMENT), A JOINTING SYSTEM WHICH ALLOWS VERTICAL DISPLACEMENT OF THE SLAB WITHOUT SEPARATION OF THE JOINTS AND CAUSING A TRIP HAZARD IS TO BE USED.
- COUNCIL'S TREE OFFICER/ARBORELIST IS TO BE CONSULTED AS TO DETERMINE ADEQUATE TORSION COVER OVER EXISTING TREE ROOTS REQUIRED PRIOR TO INSTALLATION.
- "TRIPSTOP" JOINTING SYSTEM OR EQUIVALENT SHALL BE USED IN NEW OR REPLACEMENT FOOTPATHS WHERE THE SLAB IS TO BE INSTALLED NEAR OR ADJACENT TO A TREE.
- "TRIPSTOP" OR EQUIVALENT SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES.
- THE "TRIPSTOP" 'T' PROFILE OR EQUIVALENT SHALL BE INSTALLED TO MATCH THE FOLLOWING SLAB THICKNESSES:  
(v) 15755 = 150mm THICK CONCRETE SLAB  
(vi) 15755 = 100mm THICK CONCRETE SLAB  
(vii) 15755 = 75mm THICK CONCRETE SLAB
- "TRIPSTOP" JOINTING STRIPS OR EQUIVALENT MUST BE INSTALLED FOR THE FULL DEPTH AND WIDTH OF THE SLAB.
- THESE STRIPS MUST HAVE UP TO 5mm OF CLEARANCE AT EACH END OF THE "TRIPSTOP" TO ALLOW FOR AN EDGING TOOL TO BE PLACED WITHOUT INTERFERENCE. THE "TRIPSTOP" EDGING OR EQUIVALENT MUST BE INSTALLED WITHIN A 5mm TOLERANCE OF VERTICAL.
- WHEN INSTALLED IN STRAIGHT SECTIONS OF PAVEMENT, INSTALL TO 4/- 30mm PER METER OF WIDTH FROM A RIGHT ANGLE TO THE LENGTH OF PAVEMENT.
- WHEN INSTALLED IN CURVED PAVEMENTS, INSTALL RADIAL TO THE CURVE AT 4/- 30mm PER METER FROM THE RADIAL LINE.
- "TRIPSTOP" STRIPS OR EQUIVALENT SHALL BE POSITIONED DIRECTLY IN LINE WITH THE MOST ADJACENT TREE ROOT. ONE STRIP SHALL BE PLACED IN LINE WITH THE CENTRE OF THE TREE TRUNK. CONTINUE WITH INSTALLATION OF MORE SECTIONS OUTWARDS UNTIL AT THE END OF THE DRIVE LINE.

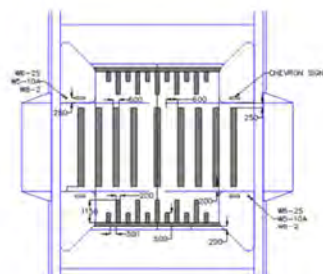
1. 18/02/2022 PRELIMINARY DESIGN	MS	LEVEL, DATUM AND CO-ORD SYSTEM	MOA**	DESIGN APPROVED	DESIGNED BY: M. SIMONS	APPROVED FOR CONSTRUCTION	DATE: 18/02/2022
2. 14/03/2022 AMENDMENTS	MS	SURVEYING DATA		DESIGNED BY: M. SIMONS	DATE: 14/03/2022	PROJ. MGR. (NAME)	DATE: 14/03/2022
3. 20/03/2022 AMENDMENTS	MS	WORK-AS-EXECUTED		INITIATED BY: P. GRAY	APPROVED BY: T. LAU	PRINCIPAL ENGINEER	
4. 20/03/2022 CONCEPT 1	MS	BY: (NAME)		INITIATED BY: P. GRAY	APPROVED BY: T. LAU	PRINCIPAL ENGINEER	
5. 20/03/2022 INITIAL DRAWINGS	MS	DATE: 20/03/2022		INITIATED BY: P. GRAY	APPROVED BY: T. LAU	PRINCIPAL ENGINEER	



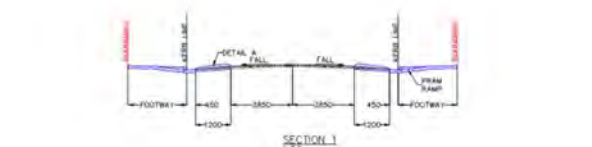
**NEW SHARED PATH**  
ABBOTT ROAD, NORTH CURL CURL  
FROM MANLY H.S. TO GRIFFIN ROAD  
DETAILS 2  
DRAWING NO. 10-ABBOTT ROAD-SHARED PATH-AMENDMENTS-6002  
REVISION 5

PLAN - ISLAND  
SITE

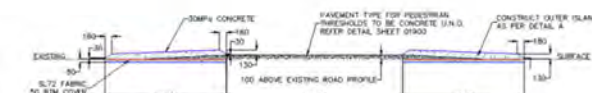
\*\* THE RMS MINIMUM WIDTH IS 3000. HOWEVER THE WIDTH MAY BE INCREASED TO 5000 FOR THE PLATFORM AND PEDESTRIAN RAMP WHERE NECESSARY BUT WILL BE SUBJECT TO SITE CONSTRAINTS. REFER TO RMS TECHNICAL DESCRIPTION FOR FURTHER GUIDANCE.



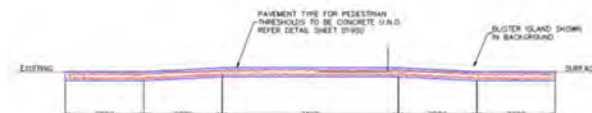
## PLAN – LINEMARKING AND SIGNAGE



SECTION

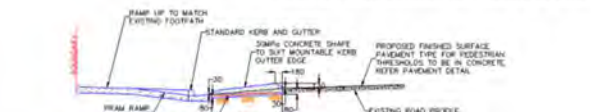


SECTION \_\_\_\_\_



SECTION

NOTE: REFER TO SHEET D1000 FOR STANDARD CONCRETE THRESHOLD DETAILS WHICH INCLUDES REINFORCEMENT, CURING, DIMENSIONS AND FINISH.



DETAIL



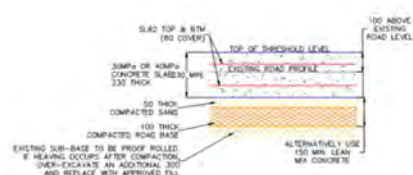
SECTION 1 — PEDESTRIAN CROSSING RAMP DETAIL

## RAISED PEDESTRIAN CROSSING NOTES

1. ALL ISLANDS ARE TO BE PAINTED WHITE WITH THERMOPLASTIC OR SMOKE.
2. PEDESTRIAN CROSSING RAMPS SHALL BE COLOURED "DARK TERRACOTTA" COLOUR "TWO" OF EQUIVALENT.
3. THE CONCRETE THRESHOLD (RAISED PLATFORM) IS TO BE COLOURED "DARK TERRACOTTA" COLOUR "TWO" OF EQUIVALENT.
4. PEDESTRIAN CROSSING RAMPS SHALL BE PAINTED WITH "TWO" OF EQUIVALENT. ALL ISLANDS ARE TO BE PAINTED WHITE WITH THERMOPLASTIC OR SMOKE.
5. ALL ISLANDS ARE TO BE PAINTED WHITE WITH THERMOPLASTIC OR SMOKE. (H-1 & 2) AS INDICATED ON THE LANE MARKING AND SIGNAGE PLAN OR OTHERWISE SPECIFIED.
6. PEDESTRIAN ACCESS RAMPS ARE TO BE CONSTRUCTED ON BOTH APPROACHES TO THE CROSSING. RECOMMENDED SLOPE IS 14 AND 16 SLOPE 14 IS TO BE USED.
7. FOR REGIONAL ROADS, REFER TO RMS GUIDELINES (RMS TECHNICAL) DIRECTION FOR HEIGHT DETAILS AND ADAPTIVE THERMOPLASTIC APPLICATION RATE.
8. KEEP ROSS ISLANDS RAMPS ARE TO BE TYPE OF KEYS OR SMOKE.

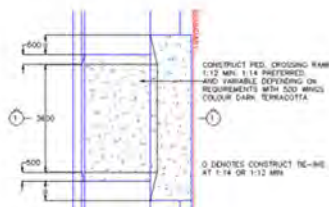
## PEDESTRIAN CROSSING SPECIAL DETAILS NOTES

- HIGH LIVING AND LOW LIVING FOOTPATHS WILL REQUIRE SPECIAL TREATMENT AND SPECIAL DESIGN CONSIDERATIONS. CONSIDERATIONS INCLUDING RAMP SLOPE OF 1 IN 12 (MINIMUM) MAXIMUM SLOPE 1 IN 8.
- EVERY CROSSING SHALL HAVE AT LEAST 10% TO 15% OF FORMALIZED FOOTPATH ON EITHER SIDE OF THE CROSSING.
- ALL CROSSINGS SHALL BE TO HAVE CHURNERS AS INDICATED ON THE LIMEWASHING AND GRAZING PLANS.
- "KEEP LEFT" SIGNS ARE TO BE PLACED ON BOTH APPROACHES IN THE CENTRE ISLAND.
- PEDESTRIAN CROSSING RAMPS ARE TO BE CONSTRUCTED ON BOTH APPROACHES TO THE CROSSING.
- PEDESTRIAN SLOPE 1 IN 18 AND MAXIMUM SLOPE 1 IN 12 WITH STRAIGHTENED GRASS 1 IN 8.
- PAVED PEDESTRIAN WALKWAYS AND PEDESTRIAN RAMPS ARE TO BE COLOURED "DARK BERRAQUOTA" (SHADE TINT ON EQUIVALENT).
- ALL CONCRETE THRESHOLDS (EXCEPT BLISTER ISLANDS) ARE TO BE COLOURED "DARK BERRAQUOTA" (SHADE TINT ON EQUIVALENT).
- WHERE OVERCROSSINGS ARE SHOWN THESE SHALL TAKE PRECEDENCE OVER SEALING.



PAVEMENT DETAIL

NOTE: ALL THRESHOLDS CONSTRUCTED IN CONCRETE WILL REQUIRE SPECIAL JOINT AND REINFORCEMENT DETAILS.

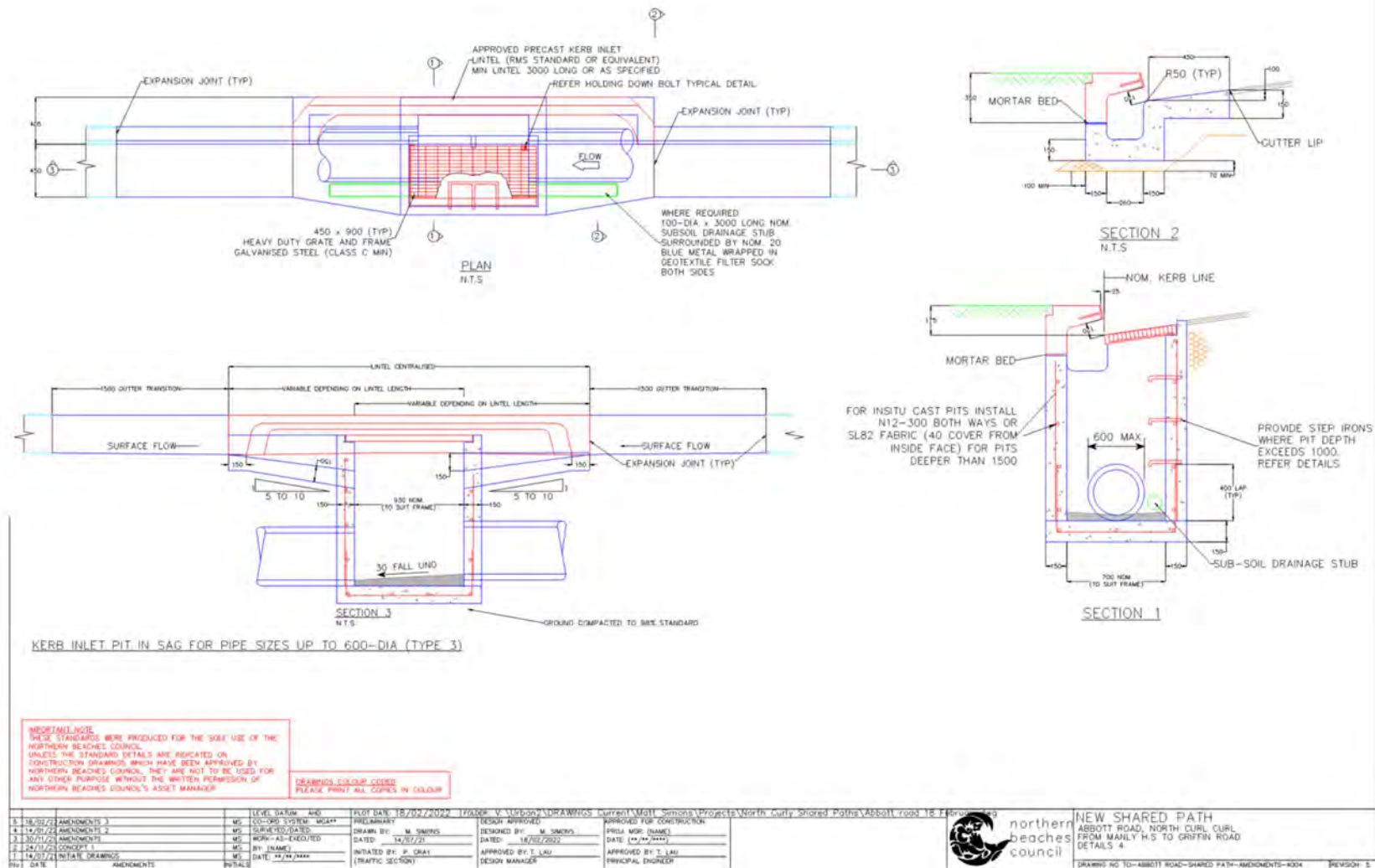


PLAN - PEDESTRIAN ACCESS RAMP

(NOTE: SAME PHOTO MAY BE SUBMITTED & REPRODUCED)

RAISED PEDESTRIAN CROSSING DETAILS  
10m TO 11m WIDE ROAD CARRIAGEWAY

[illegible]






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## Community and Stakeholder Engagement Report

### Abbott Road North Curl Curl, Shared Path

Impact level: Four

Consultation period: 23 August – 19 September 2021

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## 1. Summary

This report outlines the community and stakeholder engagement conducted as part the Abbott Road, North Curl Curl Shared Path project conducted from 23 August to 19 September 2021.

We sought comment on a proposal to build a new shared along Abbott Road between Griffith Road, North Curl Curl and Harbord Road, Brookvale.

The proposed concept plan included a new 2.5 to 3m wide concrete shared path to provide safer pedestrian and bicycle route connection to Northern Beaches Secondary College and Curl Curl North Public Schools along the Abbott Road corridor.

The feedback collected during consultation indicated a high level of support (over 70 percent) for the proposed shared path upgrade, citing the anticipated benefits for all path users. However, many comments stated a preference for the path to be located on the southern side of Abbott road adjacent to the netball courts to avoid conflicts with resident driveways. Council has revised the concept plan based on this feedback.

The proposed path will now be located on the southern side of Abbott Road between Pitt Road and Playfair Road. This location is now possible due to the coinciding works to the netball courts. Council is proceeding to detailed design based on a revised concept plan available on our Your Say project page.

Those not supportive of the proposal raised safety concerns and questioned impact on existing trees, as well as the increase of hard surfaces on the environment.

### 1.1. Key outcomes



Total unique  
responses

135



How responses  
were received

Online comment form

Completions: 129\*

Written submission

Received: 6



Do you support the  
proposed shared  
path?







■ Yes

■ No

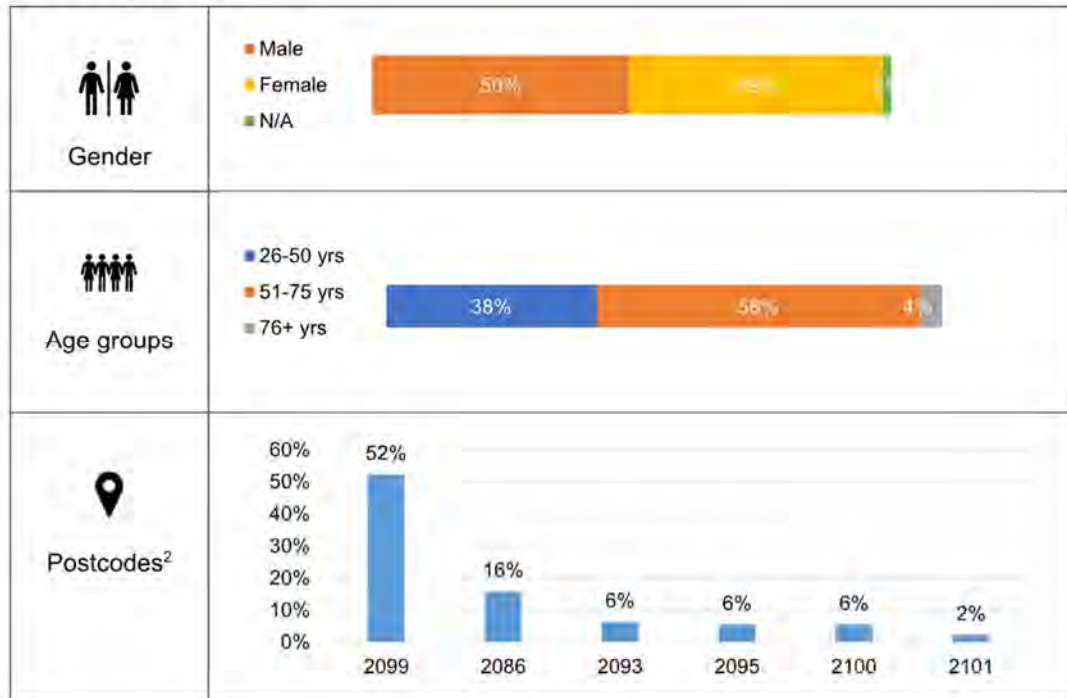
■ Neutral/prefer not to say  
n = 129  
online

\* Not every respondent made a comment in addition to answering the sentiment question

## 1.2. How we engaged

	Visitors: 507	Visits: 644	Av. time onsite: 2m 23sec
Have Your Say			
	Letterbox drop: Adjacent residents and property owners	Distribution: 89	
Print media and collateral	Site signs used: Yes	Number of signs: 12	
	Community Engagement (fortnightly) newsletter: 2 editions	Distribution: 22,000 subscribers	
Electronic direct mail (EDM)	Council (weekly) e-News: 2 editions	Distribution: 150,000 subscribers	
	Emails: <ul style="list-style-type: none"><li>• Curl Curl North Public School</li><li>• Northern Beaches Secondary College Manly Campus</li><li>• Manly Warringah Netball Association</li><li>• Curl Curl Parkrun</li><li>• Running Stars</li><li>• Manly Warringah Softball Association</li><li>• Curl Curl Knights</li><li>• Harbord Harlequins Rugby</li><li>• Curl Curl Football Club</li><li>• Manly Seaside Baseball</li><li>• Northern Beaches Cricket Council</li><li>• Manly Warringah Junior Cricket</li><li>• Friends of Curl Curl Lagoon</li></ul>		
Key stakeholder engagement	Meeting: <ul style="list-style-type: none"><li>• Friends of Curl Curl Lagoon</li></ul>		
		Attendance: 4	

### 1.3. Who responded<sup>1</sup>



<sup>1</sup> Demographic data was gathered by request only. The data represented only includes those respondents who provided this detail.

<sup>2</sup> Top 6 of 17 post code areas that respondents identified as being from.

## 2. Background

The project is in line with Council's Move Northern Beaches Transport Strategy and was identified in the Northern Beaches Bike Plan as a future connection within our safe cycling network aimed at providing additional active transport options across the Northern Beaches.

Council received funding through the Federal Stimulus – School Infrastructure Program to construct this safer pedestrian and cycle route connecting to Northern Beaches Secondary College and Curl Curl North Public School.

## 3. Engagement objectives

Community and stakeholder engagement aimed to:

- identify community and stakeholder concerns, local knowledge and values
- seek out and facilitate the involvement of those affected by or potentially interested in a project
- provide accessible information so community and stakeholders could participate in a meaningful way.

## 4. Engagement approach

Community and stakeholder engagement for the Abbott Road, North Curl Curl project was conducted over a four-week period, from 23 August 2021 to 19 September 2021.

The engagement was planned, implemented and reported in accordance with Council's [Community Engagement Matrix](#) (2017).

A project page was established on our have your say platform with information provided in an accessible and easy to read format.

The project was primarily promoted through our regular email newsletter (EDM) channels.

Feedback was captured through an online comment form embedded onto the have your say project page. The form included a question that directly asked respondents for their level of support on the proposal.

An open-field comments box provided community members a space to explain or elaborate on their support, not support or neutral sentiment as well as any other feedback they wished to contribute.

## 5. Findings

Most respondents (more than 70 percent) were supportive of the project as they felt the proposal would benefit pedestrians and bicycle riding in the area. However, a number of comments noted potential safety concerns with the path running adjacent to residential driveways. This has led to the project team re-assessing the concept design to remain on the southern side of Abbott Road between Pitt Road and Playfair Road. Detailed design has now commenced taking this amendment into account.

Comments that were not supportive of the project have been themed and responded to in Table 1 below.

**Table 1:** Issues raised and our response

Issues raised	What we heard	Council's response
Path location (Northern side of Abbott Road)	Many of the objections to the project were based around the path being located on the northern side of Abbott Road adjacent to residential properties. Concerns raised were that this proposed situation was dangerous, and the path should remain on the southern side of Abbott Road	<p>Due to the concerns from residents, the path location has been re-assessed and amended to remain on the southern side of Abbott Road between Pitt Road and Playfair Road.</p> <p>This is now achievable due to the current works to the netball courts, allowing the courts to be moved slightly which will allow adequate space for the path, court runoff and maintain existing trees and car parking.</p>
Safety – pedestrian and bicycle conflict	Concern that shared paths are dangerous or uncomfortable for pedestrians' due potential conflicts with bike riders.	<p>Transport for NSW Centre for Road Safety released a discussion paper on shared paths in 2015. The concluding remarks, based on evidence, stated that shared paths represent a relatively low safety risk.</p> <p>It is important that the shared path is designed to be as safe as possible with adequate width. The intended width of this path is 2.2 m.</p>
Safety – driveways	Concerns were raised that the path is unsafe as it is difficult for people exiting driveways to see path users.	<p>The path location has been re-assessed and altered accordingly to be located on the southern side of Abbott Road, away from residential driveways.</p> <p>However, it should be noted that utmost care should be taken when exiting driveways as the onus is on the vehicle driver to ensure they do not impede path users. There is an existing footpath in this location where people currently walk, ride wheeled recreational devices and where children (and accompanying adults) are legally allowed to ride a bicycle.</p>
Safety – crossing locations	Concerns were raised that the proposed crossing point near number 108 Abbott Road was noted to be potentially unsafe due to visibility.	The concept plan has been re-assessed and there will no longer be a crossing in this location due to the path being relocated to the southern side of Abbott Road in this section of the design.

Environment	Some respondents were concerned that more hard surfaces in the area is detrimental to the environment.	Hard surfaces would be increased, however the environmental effect of runoff due to increased hard surfaces in an existing urban street would not greatly impact the surrounding environment.
Environment	Some respondents were concerned that the proposal may result in loss of trees.	<p>An important consideration for this project is to minimise any loss of mature vegetation. The path alignment has been refined to reflect this. Over the 1.3km path project, there is one casuarina tree and one large shrub/small tree that has been identified for possible removal due to safety concerns. This will be assessed by Council Tree Officers before any decision is made on removal.</p> <p>We also expect environmental benefits through the increase of active transport options over the use of cars.</p>

During the consultation period, Council received a number of questions either through direct contact or within feedback received.

**Table 2:** Questions and Council's response

Question	Council's response
There is an existing shared path through John Fisher Park. Why is this needed?	The proposal offers another option for people to walk and cycle for recreation and transport in what is sometimes a congested area. Multiple routes offer choice and appeal to different users. Multiple routes also form loops that provide greater recreational benefits.
Why wasn't this proposed as part of the Curl Curl North Public School upgrade?	The school upgrade was undertaken by the NSW Government. The proposed shared path was previously identified in the Northern Beaches Bike Plan as a future project. Council has only recently been able to secure funding from the Federal and NSW Government to construct the path.
What about additional connections?	<p>The Northern Beaches Bike Plan identifies other proposed connections. These projects will be progressed when funding becomes available.</p> <p>A shared path connection is currently being constructed on Playfair Road, North Curl Curl.</p>

	Council is currently developing a safe cycling connection from John Fisher Park to Freshwater Village. This will connect to the existing shared path network that continues to Manly.
Will there be increased cycle racks on the new paths for people using the ovals?	Council is continually providing additional bicycle parking where required across the local government area. The priority of this project is to provide the path connection. If budget allows, bicycle parking in key locations may be provided as part of this project.
Is there sufficient space to widen the path without removing the trees? (at building near Spring Road)	Subject to detailed design, it is proposed that the trees will remain in place and a narrower path will be located between the building and Abbott Road. If the building was to be redeveloped in the future then there may be opportunity to provide a wider path in this location.
Will the proposed crossing (at eastern end of Curl Curl North Public School) increase the level of flooding during heavy rain? Will council fix the drainage to prevent flooding? Will council provide a stormwater and drainage assessment?	This location is at the low point of the catchment and hence flooding may be unavoidable at certain times. This area and the crossing treatment is subject to detailed engineering design and consideration will be placed on stormwater, drainage and flooding as part of the crossing treatment design process.

## Appendix 1 Verbatim community and stakeholder responses\*

Number	Comment
1	AN excellent plan and much needed
2	Well overdue, excellent idea. Encourages the local community to stay active and connect Curl Curl and Freshwater.
3	Great to see safe transport alternatives being constructed in line with Councils safe cycling network.
4	This would be a half decent shared path if the riders weren't forced to cross the road TWICE!!! Come on you can do better than this surely!!! Make it one path along one side all the way. Let me guess what was at stake that stopped you from doing a decent design.. car parking??? Get with the 21st century and STOP PRIORITISING CAR PARKING!!!! CARS ARE NOT THE SOLUTION. Doesn't your core planning strategy document say that? So act on it!
5	Overdue as already being used by many as such purpose as outlined in your plan. A fantastic plan that will assist with existing foot and bike traffic from all users in the area
6	Thankyou
7	I find many bike riders have a sense of entitlement and do not indicate when they are approaching from behind. Also, I have been knocked over by a learner on the shared path around Narrabeen Lakes, so I am not a fan of shared paths. There needs to be a lot more education into being mindful of others on these paths.
8	Additional path is needed to connect proposed path to bridge over lagoon near cricket nets. During winter season, only way to cycle to this pathway coming from beach direction is through netball courts that are very crowded or the long way around near softball fields. Path past Carpark near community centre would help. And when are we going to get a proper hockey field?
9	Please use the funds on sporting fields. Just tell people how to walk instead of giving them a line to stay left. Yes, I walk often around the netball and Weldon Oval (both need water), very sad how the grounds are left to injure a player one day. Also, I don't use the concrete paths as they cause knee arthritis.... yes, years of walking in the Sydney CBD on concrete.... I use the grassed footpaths, so please leave the paths for me...thanks.
10	Yes please. Cycled on this road last Friday with my wife. V car centric, not easy for occasional cyclists. Proposed share path can only improve & promote cycling.
11	We would cycle a lot more with this path, and similar initiatives.
12	Will there be increased cycle racks on these new paths for people using the ovals?
13	While the proposed path might provide safer access for students to the secondary college, I think the money would be better spent upgrading the paths along both sides of Greendale Creek. This would be more likely to attract recreational use and with a safer crossing or two on Griffin Rd, provide a better way of accessing the beach

\* Personal details have been redacted where possible. Spelling and grammatical errors have been amended only where misinterpretation or offence may be caused.

13	Please consider separated cycleways. These are a much better option for cyclists and pedestrians.
14	I do not support these concrete paths that the council is pushing everywhere , they are not appealing at all , the council is suppose to be going green not grey concrete everywhere ., so much for taking care of the environment . I do not support wide pathways on Abbott road opposite the net ball courts , that is pure madness , these so called pathways are going to be used by all sorts of bikes, scooters , skateboard many of them electric and a lot of them can move quickly causing problems for residents getting out of driveways, and it would only be a matter of time for a major accident to occur, money would be better spent on finding a new home for netball , then everyone could use the park for what is was designed for
15	Move the pathway on the netball side , not on the house sides
16	Paths should be at least 3 metres wide Speed limit for bikes 10 kph Bikes are not to be ridden across Crossing or at right angle to direction of road Riders must dismount as required by law Council to enforce in design
17	Hopefully this is just the start and it will be upgraded to a separated cycleway later. More people cycling will mean fewer cars so the space should be there.
18	Separate active transport lanes to and from a key sports, recreation and education area is absolutely needed and will be well used.
19	This will connect a number of high thoroughfare paths around the park area and will include part of the route of parkrun which will be appreciated by that group also. It is not clear however what is being proposed at the site where there is a narrow path between building and existing trees at the eastern end of abbott road - is there sufficient space to widen the path without removing the trees?
20	This would be of great benefit to cyclist and pedestrians.
21	Excellent proposal
22	The latest lock-down has highlighted how important it is to have a diverse range of paths across the Northern Beaches. Please provide lots of signs and maps (YOU ARE HERE), together with how long it will take to (say) reach the beach, so that it's easy for people to know where they are.
23	Very busy area with children, sporting activities and dog walking and cycling
24	I strongly support the bike path down Playfair Rd however feel that a wide bike path along Abbott Rd soccer fields is unnecessary. There is already the shared path along the lagoon for cyclists which is well used. Abbott rd at the rear of the cars is a high pedestrian area - always full of kids putting on soccer boots, eating, getting ready for training etc and a bike bath encouraging people riding fast along there is not needed.
25	The more NBC bike paths are better.
26	I support the path but a better solution will be a dedicated cycleway
27	There are already multiple footpaths and cycleways in this area. Please direct funding to areas which have nothing.
28	The more wide pedestrian paths the better! However, please include notices to dog owners regarding compulsory leashing of their pet and diligent collection and disposal of dog poo. Dog poo piles are everywhere on and beside paths in the Curl Curl area and many dogs are not under the proper control of their owners. Greater signage and enforcement of responsible behaviour by dog owners should be incorporated in the implementation of these new paths for the safety and enjoyment of walkers and cyclists.

29	should be great with heavy usage of pedestrians and wild kids on bikes. Fully support it
30	I think this is a great initiative and we are all for building better cycling infrastructure in Warringah. My only suggestion is to build the path on the park side along Abbott Road where possible and it seems the space is available. This negates the opportunity for a car reversing out and hitting a child unnecessarily.
31	Fully support this project. Safety of children is so important in this extremely busy sport precinct.  I particularly support the plans in the section along Abbott Road, North Curl Curl (East of Netball Courts). As an organiser of Running Stars - Curl Curl and also a regular Curl Curl parkrunner this upgrade would be greatly appreciated.
32	General bitty-ness of paths in general along the side of this road - that this plan addresses. Well frequented sportsfields need proper paths nearby like this plan has. Fully support.
33	Shared paths cause too many accidents!  Too many trees and shrubs will have to be destroyed, trees that we need so much for creation of rain, storage of CO2, shade and habitat for our unique fauna. I understand the necessity of minimisation of car reliance, however, surely there are other ways to create more possibilities for bikes and pedestrians, without the destruction of existing trees and shrubs!! Maybe minimising the width of roads, creating coves so that cars are forced to wait and let each other pass. This will be safer for all road participants as well, as driving speeds will slow down!!
34	I wanted to raise my concerns regarding the bike path and crossing on Abbott Road.  Firstly the suggested crossing outside 108 Abbott Road - Please can a member of the council come to the road and drive towards the suggested crossing in a winter sunny afternoon. As you hit the car park - you are blinded by the winter sun and the restricted area signs are regularly hit by cars as they cannot see anything approaching this area.  I am sure you will have details of the reported accidents taking out the road signs at this very spot and cannot believe that a crossing in the extremely dangerous area has been suggested. We will without doubt have a "Sophie Delazio" situation occur in due course.  I can only think that this area has been suggested as no one has pointed this out previously.  I am more than happy to meet anyone and drive the route so you can see the danger.  Secondly - the shared bike path - great idea if was on the park side of the road where there are hundreds of acres of grassland and space - no driveways etc.  Most of the houses between Lillie St and Pitt Road have fences at the front of their properties. The reason for this is because we have 3000 + people descend on us every weekend for Netball, Soccer and Rugby. So the fences offer a privacy. Whilst great to offer privacy - it is extremely difficult to reverse out of our driveways and have clear vision and have to back out very slowly. Additional foot

	<p>traffic / bike traffic on this side will again create accidents as kids will undoubtedly wizz down the hill and get hit by reversing cars. Surely - there is no question in safety that the bike path should be put on the side of the road that has no driveways and adequate space/ grass area for passers-by.</p> <p>The section of path closer to Lillie Street also has a raised high path and the street has a number of electrical pylons. Again the opposite side of the road has neither - it is crazy to consider the residential side.</p> <p>To try and get out of Abbott Road on a Saturday is absolute hell - the traffic is banked up for over a kilometre - to add an additional Hazzard for the residents to negotiate in trying to exit their driveways is just ridiculous. I ask for a member of the council to attend on a Saturday to view this for themselves.</p> <p>It is very easy to look at the street and make suggestions on a quiet midday weekday - but the reality is very different on weekends and also the issue of the winter sun is my main concern for the position suggested for the crossing. There is not a chance that an accident will not occur there. Every afternoon cars swerve at the last minute to avoid the width restriction signs - as i said previously a number do not mangle in time and go straight through - damaged all in its path. Again the council will have records to show how many times this area has been hit by cars that are blinded by the sun.</p> <p>Please feel free to contact me to discuss / walk through / drive through.</p>
35	<p>I would prefer that the whole cycleway was put on the park side of the street. The proposed crossing outside 108 should not be placed here because people speed over the hill and there are often cars running into the posts already because of the low angle of the sun in winter. There will be a fatality. Very very dangerous spot.</p> <p>In general I accept the fact that a cycleway would be helpful for the school children.</p> <p>More thought , much more thought should be put into the placement of all the crossings.</p>
36	<p>I consider that a cycle path on the northern side of Abbott Road between Pitt and Playfair Roads would be unsafe due to the number of concealed driveways on this section of the road. It would be far safer to avoid this hazard by locating the cycle path to the parkland on the southern side of Abbott Road, avoiding car contact.</p>
37	<p>Plenty of road space &amp; this will be a great piece of infrastructure, promoting clean, healthy movement.</p>
38	<p>Like the idea of a flat and safe pathway. Too many trip hazards at the moment as there is either no path or broken, dangerous paths.</p>
39	<p>I live in Freshwater and would welcome the proposal for improved cycle paths to Curl Curl. I require off road cycle paths for safety. I do not consider it safe to cycle on onroad cycleways.</p> <p>The specific proposal at Curl Curl greatly enhances the opportunity to cycle on paths, shared with pedestrians.</p> <p>This is particularly useful in this area because of the heavy use of the sporting fields, especially on weekends.</p> <p>This proposal would encourage cycling, and walking, on greatly improved paths which would benefit many families in addition to individuals such as myself. It would have flow on health benefits.</p>
40	<p>Dear Phillip,</p>


	<p>I'm writing in regard's the proposed shared path along Abbott Road, North Curl Curl.</p> <p>While I absolutely support the idea, and giving riders safe travel is such a great way to stay healthy (exercising) and get around safely, I do have some concerns I'd like to address.</p> <p>We were extremely surprised to see that the proposed path is planned to go on the house side of the road. As you would be aware, Abbott road is a high traffic and pedestrian area, being close to both sporting fields and the primary school. On Abbott Road the path goes from the field side of the road, and the pedestrian/cyclist then has to cross the road, to the house side of the road, only to cross back again at the end of the road. The danger for the pedestrians and cyclist is far greater than it needs to be, having many more roads to cross and the risk of cars coming in and out of driveways.</p> <p>Cycling along this new path (no doubt at high speed often) in front of the houses is not ideal, both from a cyclist perspective and the occupants of the house. Numerous houses have driveways which will cross the intended cycling path, creating danger to both cyclist and home owners when reversing out of driveways. Our driveway is very blind and we have a big car. We do reverse out slowly as it is, but the danger would be far greater making this path where it is proposed. Further, numerous young children live in the area. Having a cycling path across driveways creates addition danger to young children who may walk out the front of the house and onto the cycling track. Children will likely not hear or check for the bikes, creating additional risk for cyclist and small children. We have young kids, and are concerned about the additional danger this path will create for them, in what is already a high traffic area.</p> <p>Given the vast amount of space on the corresponding side of the road, it seems like a very unusual choice to put the path immediately outside of the houses, and across driveways. We would strongly suggest that the path continues along the sporting fields side of the road. If there is concern about children running in front of the bikes on the sporting fields side of the road, we would suggest a small fence is erected, to stop this from happening.</p> <p>We are a fit, healthy young family. We are in support of anything that encourages people to be active and a good path is great for that. However, we absolutely do not support the proposal at the expense of the safety of the pedestrians/cyclists and our children.</p> <p>We would suggest keeping the path on the field side of the road. Where that sits exactly, would need to be thought through. I've thought through a few ideas, which I'd be happy to share. Of course each will have a number of hurdles to address, but nothing that can't be worked through.</p> <p>I very much hope we are able to come up with an amended proposal.</p>
41	Safety for kids
42	<p>As a long term resident I support the plan but only if the entire path is located on the southern side of Abbott Road. Where the path is proposed to be on the northern side in front of homes opposite the netball courts is a dangerous idea. Most blocks are narrow and there are multiple driveways and already, now that we have a footpath, exiting your own property is so dangerous. Please note while I live in Abbott Road I live opposite the soccer fields so this does not affect me</p>

	directly but unsupervised children on bikes and scooters are already in danger.....whether their parents are with them or not! Residents should not be put under this additional stress....traffic due to sporting activities etc is already difficult to deal with.
43	<p>This is a great proposal that will benefit the curl curl community, although I think building this along Pitt Road would have provided greater benefit to the community. Most students of Curl Curl North live to the north of the school within the suburb, and cycling connections from this area are in need of safety improvement. I support further work on Pitt Rd, including connections to the Surf Slub in the East.</p> <p>I hope that no trees are required to be removed for this project. In fact, I hope that the project incorporates plans to add additional street trees along the corridor, providing shade under the path. The Northern area of John Fisher Park has such minimal tree canopy, what is there can't be lost and more should be added.</p>
44	Shared paths promote cycling as an everyday activity, not one reserved for the Lycra Brigade. Nice work!
45	Long overdue
46	<p>I support the proposed shared pathways on the Northern Beaches.</p> <p>I am in support of more crossings on Abbott Road. I am very concerned about the proposed crossing on the east side of Curl Curl Public School increasing flooding on the proposed corner of Abbott Road. Will the proposed crossing increase the level of flooding during heavy rain? Will council fix the drainage to prevent flooding? Will council provide a stormwater and drainage assessment?</p>
47	Fantastic idea - really support the proposals to allow safer bike riding for kids to get to school.
48	Letter attached
49	Great idea, great proposal and alignment, will be well utilised.
50	<p>Shared paths should have speed limits, which are enforced. The Council's FAQs states "Shared paths provide a safe passage for people wanting to ride at low speeds. The road is still available for those wanting to cycle at higher speeds." I suggest a reasonable speed limit is 10km per hour.</p> <p>Cyclists on shared paths should be required to use a proximity warning device, when passing a pedestrian in either direction.</p> <p>I suggest elevated video-cameras be installed to support compliance measures and general public safety.</p>
51	Absolutely overdue and necessary infrastructure.
52	<p>I am not against the shared cycleway only the placement and the crossings placement for these reasons. I object to the cycleway moving to the Dee Why side of the Road at 108.</p> <p>There are 13 Driveways, 5 Light posts and two Poles to be negotiated on this side of the road between S-bend at Pitt Rd and Playfair Rd.. ACCIDENT WAITING TO HAPPEN. Residents backing out of their driveways will run into these people .Freshwater side ( park side) from 108 to Community Centre only 3 driveways and 2 Light posts.</p> <p>Cycleway makes more sense on park side . LESS DANGER.</p> <p>I strongly object to the placement of the crossing at 108 also that it is to be a RAISED crossing.</p> <p>Cars wait at the Pitt Rd S-bend and then speed down the hill. SOMEONE WILL BE KILLED.</p> <p>Also strongly object to the noise a RAISED crossing will cause. THUMP, THUMP day and night.</p> <p>Our three houses effected all have our bedrooms on the front of our houses.</p>

	I have lived and paid in rates in the area for 47 years and strongly object to the proposed cycleway and crossings as proposed.
53	Construction would also enhance participation in Curl Curl Park run as the surface is more reliable and removes trip hazards behind the building
54	We already have designed cycle ways marked on the roadway. The paths are not wide enough to be shared with cyclists. It will be dangerous. We don;t need anymore concrete.
55	It will be dangerous near the netball courts where many girls come to play netball. Also there is a bike path near the lagoon so it is doubling up the pathways.
56	<p>Shared paths are supported only where there is no option to provide separate bike paths. Shared paths are not ideal. Shared paths:</p> <ol style="list-style-type: none"> <li>1. Are dangerous to cyclists and pedestrians and are not recommended by the Pedestrian Council.</li> <li>2. Do not allow bike traffic to flow consistently. It is necessary to slow constantly to avoid driveways, bus stops, pedestrians (who frequently do not keep left).</li> <li>3. Do not encourage bike commuting as they do not allow consistent speed to allow for appropriate commuting times. In addition they are unlikely to remove cyclists who are exercising from the road, again as they do not allow for the same speed and constant flow of a roadway.</li> </ol> <p>The Northern Beaches is blessed with wide roads by Sydney standards, particularly in the Freshwater, Curl Curl suburbs. Additionally, almost all housing in these areas has off road parking, and there is adequate street parking in areas like Abbott Road to remove some parking and allow for a properly separated bike path. If this can be achieved in busy cramped streets in the City of Sydney, there is no reason why the Northern Beaches Council with all its advantages cannot provide a proper separated bike network.</p>
57	For the safety of all users, Council should consider stepping away from 'shared' paths throughout the LGA - they are mostly far too narrow; instead, we need dedicated paths for pedestrians only (often walking with their little ones and/or their dogs, or are elderly - who all too often venture on 'wrong' side of the path) and we also need separate safe paths for cyclists (who tend to speed past pedestrians, rarely ringing their bells) - both pedestrians and cyclists need to learn proper path use courtesy - I have read and understand / appreciate your FAQ notes re separate cycleways but still believe separate paths is safest option by far - of alongside roads, make two way pedestrian paths on one side of the road, and two way cycleways on opposite side of the road
58	I very recently cycled with my wife and children in this area on a leisure cycling tour on the weekend, and the absence of a cycling path in this area stood out to us. I'd be great to have this added.
59	<p>Hi</p> <p>This is a great idea that will make it easier for bike riders to move about the suburbs.</p> <p>Lets make it happen so that families can get out and about on their bikes safe</p>
60	You must prohibit e bikes from using these paths and make it clear that cyclists give way to pedestrians. These are serious safety risks which will be exacerbated by making these paths more 'roadlike'.
61	This is sorely needed. Please get it done as soon as possible.
62	Dangerous for cars reversing out of driveway - would be much safer on the opposite side of the road where all the grass is and NO driveways - it's a NO BRAINER for safety.


	Pedestrian crossing - too close to brow of hill - cars zoom down as it is not a stop intersection and would not safety stop in time - very dangerous both ways in t winter and summer sun morning and evening East to West and blinds drivers along that section. Very dangerous for children or anyone wanting to cross there - idiotic
63	<p>I think the plan looks good and is well needed.</p> <p>I would look to add another safe pedestrian crossing more towards the Griffin Rd end to help all the people coming from the north side of Abbott Rd get onto the new shared path. Ideally between Spring Rd and Blackwood Rd. There are lots of people and kids who cross here to get to the skate park and to access the soccer fields, especially busy on a Saturday morning. Cars go fast, and it can be a dangerous and wide place to cross.</p> <p>Please consider this in the plan.</p> <p>Thank you.</p>
64	Please ensure that there is good vision of cars coming out of driveways. This road is extremely busy at weekends and the path crosses many driveways.. it is good though because it makes a path accessible to children wanting to ride to school.
65	<p>The junction with Pitt and Abbott is a continuation road and not a stop sign - therefore cars fly down the hill and will not have adequate breaking time - i believe it is 37 meters for a car traveling 50K - that is not safe as the crossing will be upon cars before they know it. The sun hits your vision in both directions but mostly in the afternoon and is extremely dangerous as temporarily blinds vision. The poles in the road are often knocked over - this would be a person and worse a child. A crossing is usually outside a school or row of shops or busy section - the only time this would be used is on a Saturday - the traffic will be even more backed up from the Netball, Football and rugby - bringing even more out of control congestion to an already mental situation. I guess more parking will be taken away from the residents in the vicinity of the crossing - again an absolute nightmare at the weekend for anyone who dares to try and leave the road and park again that day. The thudd / thudd each time a car crosses over the crossing will out weigh the actual usage of the crossing - the residents have to put up with so much traffic and annoying issues related to the road conditions / parking already it is crazy. Residents will have to reserve onto the crossing which they cannot see clearly due to their front fences. To sum it up - DANGEROUS &lt; DANGEROUS ,DANGEROUS- number 1 concern. Without doubt an accident will happen if a crossing is installed at the proposed position. The 2 crossings in place on Abbott Road (both outside schools are completely adequate - both on flat sections of a clear vision road and in a sensible position which is usually regularly for children crossing for school.</p>
66	Already to much road traffic and congestion for cars. Not enough bicycle traffic to warrant this!
67	<p>Shared paths are a hazard to both pedestrians and cyclists due to :</p> <p>The close proximity to the playing fields.</p> <p>The close proximity to car parking areas.</p> <p>The probable use as a spectator/team standing, sitting and temporary equipment storage during/ on playing days. Observe pedestrian/ netball players etc. behaviour on the Abbot road to Park street pathway.</p> <p>And the general lack of care by pedestrians in using the shared path and traversing the path to access their cars and to access the playing fields.</p> <p>As a regular cyclist I consider, for example, the Queenscliff to South Steyne shared path too dangerous for a cyclist to traverse. This due to similar hazards to those I anticipate will occur on the Abbot road path.</p>
68	great idea to keep everyone safe


69	Loss of parking again for the residents between Abbott and Playfair , where the residents of 17 Lillie and 72 Abbott road notified of this design before it even went into the concept design ? , having lost 4 parking spacers of Playfair and Lillie because of the yellow corner lines now this plan takes another 2 parking spacer away because of the pedestrian crossing on the Playfair /Abbott corner , bad design of a pathway Crisscrossing over Abbott numerous times , when there is an existing pathway already on the lagoon edge that would connect to the school and other pathways, it has been mentioned about extra concrete in the park , what is the difference between ripping up grass in front of houses and replacing it with concrete ,, this council is wasting our rates money , they were suppose to be giving it back , council has gone made
70	I absolutely support new and improved bicycle infrastructure. However, there is plenty of room on both sides of Abbott Road to provide segregated paths for cars, pedestrians and bicycles. This is a step in the right direction, however it's still not good enough, rather than reducing the flow and thus speed of cars, all we are doing is increasing the amount of concrete (heat islands) for a shared path. Part of your mission for this project is to reduced the Northern Beaches' reliance in cars, to truly do that, you need make cycling and walking more efficient (and safer) than driving. Widening a footpath will not achieve this goal.
71	Great to see these paths extending to a usable network, keeping kids and recreational riders off the road and safer
72	The yellow shared path construction going from Abbott Rd, up past Curl Curl Nth school to Pitt Rd, comes out onto Pitt Rd. Riding along Pitt Rd to Griffin Rd is horrible, but I use the pathway, so will this pathway be widened as well? Also, the shared path from Park St (Sth of Curl Curl lagoon) will go across the lagoon bridge and smack bang into the kiddies playground and then across a netball court to get to Abbott Rd. What is the plan for this section?
73	Excellent idea. Not sure about crossing Abbott Rd (ie dividing the path into portions on opposite side of the road), but I guess that's the only option right at the paved netball courts, unless the courts were pushed south, back towards the creek. Would be great if the path is extended south along Griffin Rd, paralleling the beach all the way to Freshwater.
74	Ver much In favour of new cycle paths safe for all family to use
75	Looks great and well needed. Why does it need to cross the road twice as that seems unsafe and may not be used consistently? It would appear safer just to use the school crossing?
76	If this project is funded though the school infrastructure program why wasn't the planed pathway submitted with the school plans , ? Why can't the proposed pathway connect with the existing pathway around the back of Hudson oval instead of ripping up more grassed areas for concreting , there is already a concrete corridor though the netball courts to connect to the school with an existing crossing and the pathway would already connect to the other massive pathway that was constructed on the Southside of the lagoon ,
77	Do you realize this exact shared path for the point A to point B location already exists in a safe location in John Fisher park - it runs from Abbott Road Manly Selective along the lagoon to the school and netball courts ? No driveways, no cars no roads, no roundabouts - please explain to me how the proposed path is safer but offers the same start and end route ?

		
	Photo of existing shared path through parkland	
78	<p>Wanting to know if Council has looked at the building of a cycle/walk bridge from Long Reef carpark near near Surf club across to Dee Why via sand hills dunes walk into Dee Why surf club then up into North Curl Curl and onto Freshwater then Manly. From Dee Why you could hook up with Dee Why to Narrabeen or across cycle bridge to Long Reef and Collaroy. Keeping people safely off the road and increasing our cycleways. Could easily be build while all the construction equipment building Long Reef surf club.</p> <p>With lockdown it highlights the desire for residents to enjoy their area...it is crowded and being loved to death. Long Reef headland presently unsafe to walk thus extra people using Narrabeen lakes walk. We are loving our outdoor time on the Northern Beaches.</p>	
79	Very much overdue due to high pedestrian/cyclist traffic.	
80	There's already too much concrete everywhere and with it will come inevitably more signage...just take a look at South Curl Curl beach parking areas for silly multiple signage Council placement.	
81	Instead of concreting everywhere and moving cycles from the roads to pedestrian paths, how about removing car parking spaces and/ or narrowing car usage on roads (to give space to cyclists) - give back spaces to pedestrians - concrete jungles are not the answer.	
82	As a resident of Abbott Road I strongly object to the proposal with regards to the section opposite the netball courts. I already struggle to come out of my driveway	

	with people walking up and down the path. It will be incredibly dangerous if bikes are added. There is plenty of space in the park, and already a path next to the lagoon. If this was widened and the surface improved this would be a far safer option than having kids walking and cycling over 17 driveways and 2 roundabouts. It also makes sense to have the entire path on the south side of the road rather than have a crossing so close to the junction with Pitt Road.
83	Safer for kids and reduces the erosion that occurs where there are no paths.
84	<p>OPPOSE IN THE STRONGEST POSSIBLE TERMS.</p> <p>Mingling cyclists with pedestrians is extremely unsafe for all pedestrians, but is especially unsafe for children and for less mobile pedestrians. All bike riders travel at speeds that cause serious / critical injuries to pedestrians but with no / minimal injury to the cyclist when they collide. Put bicycles on footpaths and you will make it impossible for children and young teenagers to walk anywhere safely by themselves.</p> <p>Keep bicycles on the road where they belong - bicycles now have sufficient safeguards built into the road rules to protect them.</p> <p>The goal must always be ABSOLUTELY NO LOSS OF TREES: trees provide essential habitat for native birds and every single tree must be protected.</p> <p>Incremental removal of trees is always wrong.</p>
85	I support the construction of this shared path to encourage active safe transport which connects to other shared paths on the Northern Beaches
86	The idea to have a cycle path along Abbott road from 110 heading east is the most dangerous. The driveways will open onto a cycle path. Multiple driveways are affected and many cyclists will be at huge risk of injury or worse. There is a free range of options on the netball fields to accommodate it. I am forcefully opposed. Also will there be street parking outside houses. 100-102 it is already difficult to get out and see safely with cars parked not to mention netball and soccer season.
87	It would be wonderful to have the pathway to safely walk or run all the way along the road.
88	Another great initiative. Fully supportive
89	UNSAFE, ALREADY A SHARED CYCLE PATH BETWEEN ABBOTT ROAD AND GRIFFIN ROAD.
90	I often walk along here and it is dark and without a footpath
91	I run along Abbott Road every weekend, a new wider footpath will make it easier and more enjoyable for both runners and walkers.
92	This is a really important upgrade to a very popular track/circuit for most local families. We use this area almost daily, and would really appreciate the upgrade proposed.
93	Fantastic! especially good because so many kids (and kids at heart) like to ride to the nearby netball courts via these routes
94	This new path should end up getting a lot of use and encourage more (older ?) people to exercise on an easy level path.
95	Abbott Rd is busy, especially near school start and finish times, and can be dangerous for cyclists. I have regularly ridden in that area but I'm becoming more reluctant to ride on Abbott Rd as it gets busier so a bike path will improve safety and encourage more cyclists.
96	<p>The proposed pathway does not provide a safe pathway for pedestrians or cyclists. The proposal to create a wider pathway on the housing side of Abbott Road creates a new issue of safe access of vehicles into and exiting their property. There is an acute angle in front of houses #s 106 to 92 from Lillie Street west towards Pitt Road junction and would not easily provide a safe flat pathway.</p> <p>There are considerable safety issues regarding vehicular access / exiting houses,</p>

	<p>especially when reversing with trailers, boats, caravans in tow. Children often ride quickly and do not consider safe crossing of driveways.</p> <p>There is a perfectly good shared pedestrian &amp; cycle pathway on the northern and southern sides of the Greendale Creek &amp; Curl Curl lagoon currently in use and providing a safe pathway between Harbord Road and Griffin Road.</p>
97	<p>Dear Phillip,</p> <p>I had to email this as I wasn't able to attach photo. Please check email.</p> <p>For safety reasons I'm dismayed to hear of the proposed shared path along Abbott Road, North Curl Curl. I have attached a photo to explain my thoughts below.</p> <p>While I absolutely support the idea of a cycle path, giving riders a shared cycle track is such a great way for families to stay healthy and get around safely, however, I have grave reservations about your proposal where children would have to cross 4 roads and the cycleway also going across some 20 driveways (where cars ALL HAVE TO REVERSE OUT!! When a much safer alternative is available. Given the vast amount of space on the sporting field side of the road, it seems like a very unusual choice to put the path immediately in front of houses</p> <p>My suggestion to stay on the sporting field is perfectly do-able but would need a bit of clever thought, like going in between trees and around a tree root (all detours children would love), but the advantages is that it WOULD NOT require crossing any roads, and only need to go across 2 carpark entrance driveways, a much safer and desirable alternative, and a win win.</p> <p>If there is concern about children running in front of the bikes on the sporting fields side of the road, I am sure if you felt it was dire you could come up with a way to safeguard this. It would be far preferable than a child getting knocked off their bike while crossing a driveway or a road, which is much more likely as many children get knocked over in this way many times each year.</p> <p>The first part of Abbott road where it joins Harbord road, your choice is perfect to have it off the sporting fields, but then when Abbott Road goes off to the right to continue to follow the sporting fields your suggestion is to get the children to cross the busy road on a blind corner to then travel on the North side of Abbot Road which is the house side in front of the houses no's 72 – 110. Having to then cross 4 roads (3 of which can be very busy) and a total of 20 driveways!!</p> <p>PLEASE keep the cycle path on the sporting field side of the road all the way from Harbord Road to Griffin Road. I can see no problems (that cant be solved) there might be a tricky bit travelling very close to the Netball courts (where I have drawn the orange line in the photo below, there might need to be a bit more thought for just that short stretch in front of the school, certainly not unsolvable, as this path (with the zebra lines is 2.4m wide). Whereas the path from the CC Youth &amp; C Centre to Griffin Road is only 1.5m wide. Perhaps there could be a Road sign that asks bike riders to dismount and walk if there is a netball game in progress. Or even better traffic lights that the Netball umpire could turn red on the few occasions a game is in progress, so they would have to dismount and walk. Kids would love that.</p> <p>I very much hope someone will seriously look at an amended proposal. I have spend a lot of time down at the courts taking measurements and looking at how it</p>

	<p>would work, so please feel free to contact me if you have any questions.</p> <p>I have attached a photo the below dot points explain the markings on it.</p> <ul style="list-style-type: none"> <li>• Yellow line shows my proposed route for the bike track all on the same side of the sporting fields (sorry for wobbly line but I found it hard to do ?)</li> <li>• The short Orange line is the only section of the whole track under my proposal that might be a bit tricky, but solvable.</li> <li>• The 4 short blue lines show where children would have to cross the road under your proposal.</li> </ul> 
98	<p>Great plans to improve safety for our children and other users!</p> <p>Bikes and pedestrians should never use the same thoroughfare, as many bikers disregard pedestrian and dog safety by speeding and not ringing their bells. Pedestrians cannot enjoy their walks as they are having to constantly look behind them for speeding bikes.</p>
99	<p>Bells on bikes should be mandatory as well as bike registration.</p>
100	<p>Whilst I support shared paths, I don't support this current concept plan for the following reasons:</p> <p>The section of path between 108-60 Abbott Road should be located on the park side to provide one continuous path, eliminating all potential safety risks and provide users with a more enjoyable and safe experience. The current location of this section will cross 18 resident driveways, two roads Lillie &amp; Playfair Roads, and requires pathway users to cross at 108 Abbott Road and then again at 60 Abbott Road to ensure continuous use. This current plan could open the council and residents up to liability risk should an incident occur.</p> <p>The raised crossing proposed at 108 Abbott Road would be directly at the foot of a hill. This location would be extremely dangerous for users and drivers, as cars already come down that hill at speed when turning into Abbott Road from Pitt Road. The western afternoon sun when driving from east to west on Abbott Road is an existing daily hazard for drivers. Adding in the need to look for crossing pedestrians would be a major safety issue as there have already been numerous accidents directly out front of 106-110 Abbott Road over the years. An incident will occur within the first year of installation if a crossing is placed here.</p> <p>All electricity poles / telephone cables in the section of path between 108-60 Abbott Road are located on that side of the road providing an additional hazard for users. When any electricity/telephone maintenance needs to take place, this section of pathway would be unusable for a given period of time.</p>

	<p>Traffic congestion on Abbott Road is already high during the week and especially on weekends when sporting activities take place. By adding in four(4) new crossings in that section of road between 108-60 Abbott Road will only add to the existing traffic congestion problems, with cars continually having to stop for pedestrians/users of the pathway.</p> <p>Project cost saving. By placing the shared path on the park side between 108-60 Abbott Road this would remove the need for four (4) new crossings which would be a cost saving for the project and money council could use to fund other projects in the same area such as the Playfair Road proposed shared path.</p>
101	<p>The bike path should continue on the South side of Abbott Rd between the netball courts and Reub Hudson oval. This will avoid 4 x road crossings each of which will deter cyclists from using the bike path and increase the risks of collisions with vehicles. The proposed route would also cross 13 or 14 driveways all of which increase risks for cyclists as cars enter and exit private properties. On the South side there is one car park entry to negotiate. This proposal does not really consider what will be practical for cyclists.</p>
102	<p>please included the two suggested extensions to proposed works to create a complete loop around Curl Curl lagoon that is safe and family friendly (similar to Narrabeen Lake loop). See attached image with mark ups.</p> 
103	<p>My main concern is safety: both pedestrians and cyclists . I walk Abbott Rd and through the park from Harbord Rd to Griffin Rd daily; very few cyclists will give a warning that they are about to overtake you. I always thank those who do so. Education should be a priority, most of us can ride a bike but we should do so safely and with consideration for others, and learn the rules (I realise that we are being bombarded with rules and regulations at the moment but this should be a basic requirement for all) . How many cyclists dismount when using a PEDESTRIAN crossing if there is no bicycle crossing light? The following website is very informative, perhaps some of the relevant points could be uploaded to the council website and/or included with our next rates notice. <a href="https://www.nsw.gov.au/topics/roads-safety-and-rules/bicycle-safety...">https://www.nsw.gov.au/topics/roads-safety-and-rules/bicycle-safety...</a> Regards</p>
104	<p>Please incorporate some form of shared path along Griffin Rd to make this a loop joining the existing shared path. Even better, please extend that to create a decent shared path between Curl Curl beach and Dee Why. Griffin Rd is really busy and it's the only on road cycle between Manly and Dee Why.</p>

105	<p>A path on the southern (Sports field / netball courts) side of Abbott Rd makes sense but I definitely do NOT agree with the proposed path (Photomontage2) portion on the northern side of Abbott from Playfair to Pitt Rd.</p> <ul style="list-style-type: none"> <li>- Volume of traffic makes it dangerous for children to be crossing Abbott Rd, especially on netball / sport days.</li> <li>- Adding crossings will result in heavier traffic build-up.</li> <li>- Eastern sun (mornings) and western sun (afternoons) impacts driver vision along Abbott Rd now. By adding road crossings, increases the chance of pedestrian/vehicle accidents.</li> <li>- The flow of the path will be maintained if path is continued on the southern side of Abbott Rd. No reason why it could not be continued inside the existing fence for that portion. There was historical opposition to further hard surfaces within the park, but that was prior to the new hardcourts being constructed.</li> </ul>
106	<p>Thank you for the opportunity to comment on the proposed Curl Curl to Freshwater shared path. Bicycle NSW has been the peak bicycle advocacy group now in NSW for over forty-five years, and has over 30 affiliated local Bicycle User Groups.</p> <p>Our mission is to make cycling better for everyone in NSW, and we support improvements to the pedestrian environment and advocate for new cycling routes that incorporate dedicated paths within the road environment and in green corridors, providing connections to jobs, schools and services for daily transport and recreation trips. Cycling provides a healthy, congestion-reducing, low-carbon form of travel that is quiet, efficient and attractive for all ages with the correct infrastructure design.</p> <p>We applaud Northern Beaches Council's efforts to roll-out the cycling network developed for the Northern Beaches Bike Plan 2020.</p> <p>Bicycle NSW generally supports bicycle infrastructure that is separated from pedestrians. As this is designated as a local route, it runs largely along parkland and there are other east-west routes to create sufficient capacity, we consider a shared path to be appropriate.</p> <p>However, we urge the Council to have braver discussions with the community and sporting organisations to avoid moving the shared path to the north side of Abbott Road between Pitt and Playfair - if it is constructed here, it must be considered a temporary measure. Negotiating 3 roundabouts and multiple driveways is unsafe and southern side would be much better for bicycles.</p> <p>Both TfNSW and Northern Beaches Council have policies that prioritise space for active travel over parking. Please take this opportunity to create new infrastructure that aligns with these policies. There will never be enough parking for netball. The loss of a few spaces (perhaps by turning the right angle spots to parallel) will encourage the modal shift needed to meet climate, health and liveability imperatives as more families cycle to sporting fixtures.</p> <p>Kind regards, Bike Planner Bicycle NSW</p>
107	Duplicate submission
108	See file

109	After speaking to North Curl Curl School parents and the neighbors this weekend - it appears no one would allow their children to ride on the residential side of the road due to the 17 driveways and therefore making this a ridiculous option. Please listen to the people that live and breathe the street 24 hours a day 365 days a year - or you will have a serious accident on your hands.
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## Submission attachments – Via Your Say

Dear Northern Beaches Council

### Re: Abbott Road, North Curl Curl Shared Path

As owners and residents of XXX Abbott Road, North Curl Curl, we welcome the idea of increased shared path networks to improve connectivity and safety, per the Northern Beaches Council Transport Strategy 2038.

However, we oppose the current concept of a wider path on Abbott Road between Pitt Road and Playfair Road on the grounds that it does not provide a safe environment for all users. The *Our Active Travel Future Directions* of the Northern Beaches Council Transport Strategy 2038 states:

2. *Deliver safe, active travel across all modes of transport for school-aged children and young people.*
3. *Provide a safe environment, both on and off-road for all users and end of trip facilities to make it a realistic option for commuting.*

The current concept will not deliver, nor provide a safe environment for all users between Pitt Road and Playfair Road due to:

- a) Reduced user safety.

Like many of our neighbours, we are required to reverse out of our driveway – twice per day at a minimum – which already impedes clear sight of path users. Added to this, the high front retaining wall of our neighbour's property limits our view even further, making it even more difficult to see path users until we are almost on the path. School-aged children and young people currently use the path to ride bicycles, scooters and skateboards at high speed without any awareness of cars entering/leaving properties.

- b) There are a number of existing large power poles on the path. Increased traffic on the path will make it difficult to navigate around the power poles, particularly (i) when some poles will be in the centre of the proposed widened path, and (ii) where the ground is not level [see (c) below].

- c) The land on either side of the existing path is currently not level in areas along Abbott Road.

Example One: between the existing path and properties at 98 - 104 Abbott Road. Extending the width of the path towards these properties will increase the height between the path and properties, therefore increasing run-off.

Example Two: between the existing path and the road between 94 Abbott Road and Lillie Street. Extending the width of the path will increase the height of the drop from the path to the road. This will reduce user safety, especially as user traffic increases, and could lead to children falling/riding their bicycles onto the road and oncoming traffic.

- d) Lighting. The Strategy states: *Consider proper lighting along footpaths and cycle ways. Currently there are too many dark footpaths which is unsafe for women, children and cyclists.* Any proposed additional lighting beyond the current street lighting should be on the field side, away from houses.

In summary, we feel that increased user traffic on the proposed Abbott Road path between Pitt Road and Playfair Road, particularly school-aged children on bicycles, scooters and skateboards, will not provide a safe environment for all users. We propose that the shared path is on the field side, opposite the properties between Pitt Road and Playfair Road, in line with the remainder of the proposed path.

Regards

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#### Share Path: Curl Curl – Abbott Road

I support the proposed location of the shared path east of Curl Curl Youth and Community Centre, but not the proposed location to the west of this building (Abbott Road West).

I would prefer that the shared path in Abbott Road west is located adjacent to the northern boundary of John Fisher Park.

This is a preferred location for reasons include safety, recreation and continuity with the shared path in Abbott Road east.

#### SAFETY

The route on the northern side of Abbott Road requires:

Crossing Abbott Road at two new locations (E and W of existing pedestrian crossing).

Crossing Playfair Street and Lillie Street (a minor road).

Traversing numerous private driveways.

Safety concerns due to road crossings and private driveways.

Many children will be using the share path, so it is important to avoid or reduce conflict with traffic.

The John Fisher Park route crosses two car park entrances but requires no road crossings.

#### PROXIMITY to ROAD

The shared path on the northern side of Abbott Road is very near to the kerb, as the verge has little space for a setback from the roadway. This reduces the safety of the shared path, particularly for children.

Safety concerns and conflict with cars increases the need for vigilance, particularly for parents.

This increases stress levels and reduces the relaxation / recreation benefits of walking and cycling.

#### TRAFFIC DISRUPTION

Two new road crossings are required across Abbott Road.

Frequent use of the crossings in Abbott Road will disrupt traffic.

During busy times this could cause of congestion along this route.

For many cyclists and pedestrians, crossing to the northern side of Abbott Road will be an unnecessary diversion.

#### NETBALL COURTS

Used for about 6 months mainly on Saturdays.

That is about 26 days of the year, whereas bike path used every day of the year.

If necessary, shift boundary of grassed courts to south to accommodate path.

## TREES

Existing trees provide some shade on northern boundary of the Park.

Leave space for additional tree planting along the northern boundary of the park.

Throughout the year many days are sunny as well as hot, so shade provides relief.

## COMMENT

Based on the Frequently Asked Questions (FAQ) reference below, the location of the shared path is based partly on constraints and preliminary feedback. However, from observation, the impact of a shared path along the northern boundary of the Park on the netball courts, existing vegetation and car parking appears to be minimal. The FAQ does not give specifications re the constraints or if there is scope for adjustment.

The advantages of an alternative route via the Park warrant consideration.

Reasons:

1. Increase in hard surfaces at the edge of the Park is marginal, but has benefit for recreation.
2. Minimal conflict with netball courts and avoids N-S traverse across netball courts.

Having walked the route, there appears to be adequate space for a shared path inside the northern boundary of the Park for most of the route west of the Curl Curl Youth and Community Centre.

Between Curl Curl Sports Centre and Creative Space:

Roadside parking is provided within this section, so space is required for pedestrian access within the boundary of the Park.

Between Creative Space and opposite Manly High School

A preferred route is south of the existing fence in John Fisher Park, not alongside the road. The advantages include visual amenity, potential shade and safety. Walking or cycling beside a roadway provides active transport and exercise but is not so conducive to health and recreation.

In conclusion, consider the feasibility and benefits of relocating the proposed route of the shared path in Abbott Road west within the boundary of the Park.

## REFERENCE:

### FAQ

[Why is the path not in the Park along Abbott Road between Pitt Road and Playfair Road?](#)

*It is difficult to locate a path of adequate width (2.5m) in this section of the park without impacting netball courts, existing vegetation and car parking.*

*There has also been some opposition to further hard surfaces within the park, leading to the preferred location being located on the northern side of Abbott Road.*

[There are already east-west connections along John Fisher Park, why do we need another?](#)

*The proposed path provides connections along the northern boundary of the park. This location provides more direct access to schools and offers routes along the road corridor which benefit from improved visibility and street lighting.*

*Additional routes also offer greater recreational value by creating more opportunities for walking, jogging and cycling loops within the park.*

**Unique submissions received – Via Email**

Hi Phillip

I've just been reading the proposal for the shared cycle path on Abbott road.  
In general I think it's a great addition to the area, especially between harboard road and the turn off to the netball courts.

I do however have reservations on the section between pit road and playfair road.  
I live at 88 abbott road on the corner of Lillie street.

Increasing the width and increased cycle traffic across all the driveways on this section of road I think is not going to result in a very safe path.  
I already stress when my son zooms down the path – as someone could easily be reversing out a driveway and not notice a cyclist.  
Additionally there are quite a few electricity poles in the path already, which I assume cannot be moved, so these will create hazards as well when there are more cyclists on the path.  
Finally between Lillie street and Pitt road, the path is quite high in sections again I think creating additional safety concerns when cycling with young kids.

The path along the soccer fields is really nice to have the kids on as you don't have to worry about cars and driveways and they can easily go onto the grass to pass other users.

I would really like to request you to see if anything can be done to allow the path to stay along the park end.  
Netball season is only a few month a year – whereas the cycle path will be used all the time, so it would be great to have it done right without compromises.  
I think this is a great opportunity to increase the amenities in the area, so every effort should be made to do it in such a way that will maximise benefit for the long term.

Thanks very much

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Good Morning Phillip,

I wanted to share some additional thoughts I have on this matter.

Stormwater. In addition to the concerns raised below, I do believe that widening the path on this section will put additional strain on the stormwater system due to additional run off. Having the path along the fields will increase the hard surface, but at least any runoff will be onto the grass.

Crossing at Playfair road. I know the school raised concerns of this as it will increase the amount of traffic backup during drop off and pickup times. Having the path on the field side, will allow the use of the existing pedestrian crossing in front of the school entrance.

I do however think that putting in a pedestrian crossing across the current narrowing near no 108, and a new crossing on the bend near no 60 will be beneficial on two fronts.  
Firstly allowing pedestrian and cyclists to safely cross onto the new path, but also serve as a much needed traffic calming device. As you may know there have been many concerns of speeding on Abbott road, and these I feel will go a long way in addressing this longstanding issue.

Thanks again

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Hi Phillip,  
Thanks for talking to me about the proposed shared path/Abbott Road, North Curl Curl.  
I've attached some photos of outside 60 Abbott Road when it floods. Please take this into consideration when planning the new crossing.  
Kind regards,





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Hello Phillip,

I have had a look at the proposed shared path on Abbott Road, North Curl Curl.

I have major concerns over the section from the junction of Pitt and Abbott roads the path crossing over to the northern side of the road and then switching back over to the southern side of the road.

I can't work out how you plan on widening the path to accommodate cyclists. This is a heavy pedestrian section most of the day everyday and with a lot of driveways, I can't see how this can be done safely. As it is now, the cyclists come flying down this strip very fast and don't use their bells to indicate they are coming. I would have thought that it made more sense to make a cycle path on the southern side of the road all the way from Harbord road to Griffin road.

There is already a path that cyclists use down by the lagoon so why can't this be made into a shared path and keep them off the roads away from the houses.

In regard to Playfair Road, again this is a high pedestrian area all day everyday. I regularly walk up to the shops so I am very familiar with this street. I'm not sure how you could make that a shared cycle path safely. I am very much against the proposed plan.

Regards,  
Abbott Road,  
North Curl Curl

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## HAVE YOUR SAY SUBMISSION

The concept plan is well conceived with useful FAQs and it has my full support. However, I believe there are opportunities to make it even better.

Two of the cogent reasons given for the project are:

- Enhanced walking and cycling connectivity to schools (with Federal stimulus funding) and
- Family oriented additional loop path options, around Harbord Lagoon/Greendale Creek.

Accordingly, I posit two additional links.

1. In my submission to the Bike Plan 2 years ago, I suggested that the path which terminates at the amenities block near Mike Pawley Oval, be continued for just over 200m to the Freshwater Campus, where it could seamlessly link with the **existing** shared path on Harbord Rd. There was already a clear preferred rough path, but since Covid it has been far more heavily used and had become much wider, as the attached photos attest.  
The first was taken from the amenities block heading W, while the other two were shot from the existing path next to Freshwater Campus car park.

2. For some reason the proposed new shared path on Griffin terminates at the pedestrian refuge crossing (in itself poorly designed, involving the removal of 2 bike lanes and its corollary - the creation of a hazardous squeeze point for cyclists). Why doesn't the widened path extend for less than 200m, utilising the existing footpath (see screen shot) to link with the *existing* path in John Fisher Park, adjacent to the well designed, bike friendly pedestrian refuge?



These extensions would satisfy both the criteria (school and loop) cited for the project and would mean that not only would there be a complete loop of Harbord Lagoon/Greendale Creek for the first time, but there would be multiple safe, off road circuits for pedestrians, cyclists and family groups. Council's own aerial map below graphically depicts where the 'missing links' could be integrated, .

I'm certain that council planners will also have noticed these opportunities, so I do understand that funding is always an issue in such projects. I suggest, however, that it would be cost effective with disproportionate public benefit, to complete all the potential links during the *one* enterprise.

A key stated purpose of the Bike Plan is to expand the N Beaches cycling network and especially to link existing bike/shared paths and bike lanes. Improved connectivity encourages greater public usage and underpins Council's active transport policies. An enlarged project would achieve all these objectives.

As well as encouraging other N Beaches' cyclists to comment, I am also copying the 3 Crs who represent the Curl Curl Ward, because I believe this (expanded) project will be of great public benefit. If realised in full, I anticipate it would quickly be seen as a shorter version of the immensely popular circuit of Narrabeen Lagoon.

Please see attached signatures of all the residents along the stretch of Abbott Road. We believe that installing a shared bath path on the residential side of the road where there are 17 driveways, 18 poles, 2 roads and 2 roundabouts would not be a safe option and that installing on the opposite side of the road where there are zero driveways, zero poles, zero roads to cross and zero roundabouts would be a much safer option or use the existing shared cycle path that runs from Abbott Road to Griffin Road along the lagoon.

I have tried to upload the pages on the council (have your say) but only allows one page at a time -I will continue to try.

Kind regards

**(Council Note: Document attached with 30 signatures. Not included due to privacy).**

Document administration	
Version	4.0
Date	22 October 2021
Status	FINAL
Related Projects	Northern Beaches Bike Plan

<b>ITEM 4.6</b>	<b>AUMUNA ROAD – LAROO ROAD, TERREY HILLS – ‘GIVE WAY’ CONTROL</b>
<b>REPORTING OFFICER</b>	<b>ENGINEER - TRAFFIC</b>
<b>TRIM FILE REF</b>	<b>2022/081577</b>
<b>ATTACHMENTS</b>	<b>1 Aumuna Road – Larool Road, Terrey Hills – Plan</b>

**GEOCODES: -33.689229, 151.216840**

## **REPORT**

### **BACKGROUND**

Council has received concerns from residents regarding motorists cutting the corner at the intersection of Aumuna Road / Larool Road, Terrey Hills and creating potential risks. They requested Council to review the intersection control.

### **LOCATION**

- Aumuna Road and Larool Road are local roads with a speed limit of 50km/h. Furthermore, they are sealed roads however, do not have kerbs & gutters.
- The intersection of Aumuna Road and Larool Road is a ‘T’ intersection.

### **ISSUES**

- Motorists cutting the corner at the intersection of Aumuna Road / Larool Road, Terrey Hills, and creating potential risks.
- Default ‘T’ intersection control applies currently between the intersection of Aumuna Road and Larool Road.
- Visibility is compromised a bit due to the alignment of roads. However, due to low traffic volume, it is not a major issue.

### **PROPOSAL**

Council has undertaken a review of the above location and proposes a ‘GIVE WAY’ control with dividing barrier lines (BB) and separation lines (S1) at the intersection to enhance safety.

The Proposal is shown in Attachment:1 and includes the following:

- Install ‘Give Way’ sign with ‘Give Way Line (TB) & ‘Transverse Line (TB1).
- Installing 10m Dividing Barrier Lines (BB) on Larool Road.
- Installing 30m Separation Lines (S1) on each approach of Aumuna Road.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- As the proposal improves the intersection control, pedestrian and people cycling safety will be improved.
- The proposal does not affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

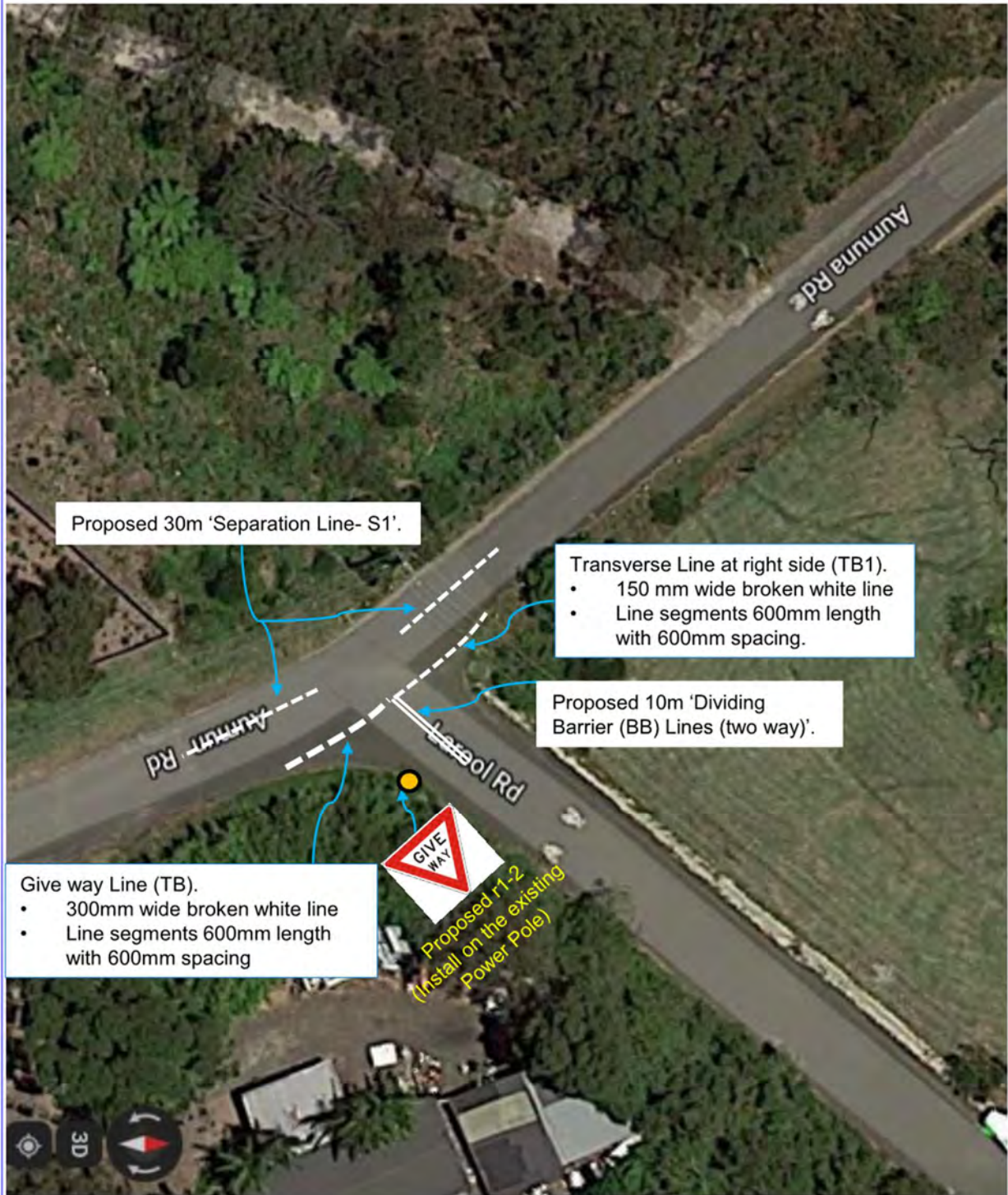
Consultation letters have been distributed to 6 properties within the immediate vicinity of the location providing notification of the proposed changes. No submissions were received at the time of reporting.



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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the:

- A. Installation of 'Give Way' sign with 'Give Way Line (TB) & 'Transverse Line (TB1).
- B. Installing 10m Dividing Barrier Lines (BB) on Larool Road.
- C. Installing 30m Separation Lines (S1) on each approach of Aumuna Road.



	PROPOSAL		 northern beaches council
	Aumuna Road – Larool Road, Terrey Hills 'Give Way' control		
	Drawn VS	Approved 	

<b>ITEM 4.7</b>	<b>STUART STREET, MANLY - NO PARKING AND NO STOPPING</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC ENGINEER</b>
<b>TRIM FILE REF</b>	<b>2022/121702</b>
<b>ATTACHMENTS</b>	<b>1 Stuart Street, Manly - Plan</b> <b>2 Table of Consultation</b>

**GEOCODES: -33.807474, 151.288400**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding difficulties accessing driveways and parking congestion on Stuart Street, Manly. Parked cars on both sides of the street reduce the road width and increase the chance of a collision as vehicles manoeuvre through narrow sections or to and from driveways.

### **LOCATION**

- Stuart Street is a local road carrying low volumes of traffic. It is approximately 7m in width, with parking permitted on both sides of the road.
- A cul-de-sac is located at the end of the street which has existing 'No Stopping' restrictions beginning outside No.99 and closes on the other side of the turning area.
- Adjacent land uses consist of low-medium density housing.
- Little Manly Point carpark which caters for visitors to Little Manly Beach, is situated on the western side of Stuart Street with access from the intersection of Stuart Street and Carey Street.

### **ISSUES**

- When vehicles are parked on both sides of the road, the available carriageway width is reduced to around 3m in width. This makes access for vehicles to and from their driveways difficult and increases the chance of a collision as vehicles manoeuvre through narrow sections.
- On weekends, particularly in summer, Little Manly Beach is a popular destination, and all available parking in the street on both sides of the road is often occupied. This can make access to and from driveways difficult and makes turning around in the street difficult for drivers unable to find a parking space.

### **PROPOSAL**

Council has undertaken a review of the above location and proposes to install:

- A No Parking restriction on the north side of the street between the driveway serving No.16 Carey Street and the existing No Stopping sign in the cul-de-sac.
- A statutory 10m length of No Stopping on the east side of Stuart Street south of its intersection with Carey Street.

- A No Stopping restriction across the Little Manly Point carpark's driveway. Relocation of the existing 7P Permit Parking Sign from the power pole to approximately 10m from the carpark's driveway is also proposed.

These installations will assist in improving access for local residents and reducing the potential for property damage for all vehicle owners.

## PEDESTRIAN AND CYCLING IMPACT STATEMENT

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

## CONSULTATION

Consultation letters have been distributed to 227 properties within the immediate vicinity of the location, providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

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## RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee supports:

- A. A No Parking restriction on the north side of the street between the driveway serving No.16 Carey Street and the existing No Stopping sign in the cul-de-sac.
- B. A statutory 10m length of No Stopping on the east side of Stuart Street south of its intersection with Carey Street.
- C. A No Stopping restriction across the Little Manly Point carpark's driveway. Relocation of the existing 7P Permit Parking Sign from the power pole to approximately 10m from the carpark's driveway is also proposed.



## Table of Consultation

<b>Address</b>	<b>Stuart Street, Manly</b>
<b>Proposal</b>	<b>No Parking &amp; No Stopping</b>

<b>Properties Consulted</b>	227
<b>Responses Received</b>	11
<b>Support</b>	2
<b>Conditional Support</b>	3
<b>Do Not Support</b>	6

Issue	Resident Comment	Council Response
Loss of parking	Several residents raised concerns about loss of parking	The proposal will result in a loss of 7 "parking" spaces however due to the close spacing of driveways all but the smallest of cars can only park in these gaps if they encroach on an adjacent driveway. All those opposing the proposal live outside the most impacted section of Stuart St i.e not within the section south of Carey St
Expand the proposed length of No Parking	Parking between the driveways serving No.s 91 to 95 is not possible without preventing access to or from those driveways and the proposed No Parking should be extended to incorporate this length of kerb	Feedback from residents of No.s 91-95 was consistent in this regard. The plan has been amended to incorporate their request.
Choke point adjacent to No.16	Parking on both sides of the road adjacent to the Stuart Street frontage of No.16 Carey Street creates a choke point and should also be No Parking on the north side	Statutory No Stopping will be signposted to prevent parking too close to the corner however parking in this section does not impact driveway access and vehicles can pull over to let an opposing vehicle pass.

<b>ITEM 4.8</b>	<b>UPPER CLIFFORD AVENUE, FAIRLIGHT - NO STOPPING UNBROKEN YELLOW KERB LINE</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC ENGINEER</b>
<b>TRIM FILE REF</b>	<b>2022/122763</b>
<b>ATTACHMENTS</b>	<b>1 Plan</b> <b>2 Table of Consultation</b>

**GEOCODES: -33.797492, 151.272579**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding the parking of vehicles on the bends in the vicinity of Ashely Parade and Upper Clifford Avenue. This impacts upon access, particularly to and from the unnamed lane passing between No's 38 & 40 Upper Clifford Avenue and leading to No.2 Ashley Parade.

### **LOCATION**

Upper Clifford Avenue is a local road of 4.7m in width, carrying low volumes of two-way traffic. This road meets Ashley Parade on its western side and an unnamed lane on its eastern side. Parking is permitted on the north side of Upper Cliff Ave and the west side of the unnamed lane.

### **ISSUES**

- Residents are concerned about the possibility of cars colliding with the parked vehicles, particularly on the bends in the vicinity of Ashely Parade and Upper Clifford Avenue.
- Parking is permitted on the northern side of Upper Clifford Avenue and the western side of the unnamed lane, leading to sightline issues at side streets, particularly if vehicles park within the statutory 10m.

### **PROPOSAL**

Council has undertaken a review of the location and issues and proposes to introduce a No Stopping Unbroken Yellow Kerb Line approximately 20 metres in length on the inside radius of the bend outside No.40 Upper Clifford Avenue, Fairlight.

### **PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

Consultation letters have been distributed to 184 properties within the immediate vicinity of the location, providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the:

- A. Installation of 20m No Stopping Unbroken Yellow Kerb Line on the inside radius of the bend outside No.40 Upper Clifford Avenue, Fairlight



### Table of Consultation

<b>Address</b>	Upper Clifford Avenue, Fairlight
<b>Proposal</b>	No Stopping Yellow Line

<b>Properties Consulted</b>	184
<b>Responses Received</b>	7
<b>Support</b>	4
<b>Conditional Support</b>	1
<b>Do Not Support</b>	2

Issue	Resident Comment	Council Response
Loss of parking	Two responses have raised concerns about the resultant loss of parking	Any parking that is "lost" is on the inside radius of a tight narrow 90 degree bend in Upper Clifford Road. Parked vehicles on this bend prevent access and inhibit sight lines
Vehicular and pedestrian access	Residents have commented that vehicles have been parking on this bend for several years where they block vehicular and/or pedestrian access	Noted, this is the reason the restrictions are being proposed
Length of restriction	The proposed 20m length of No Stopping should be reduced to 10m to reduce the impact on parking	The 20m length allows for the standard 10m length of No Stopping on either approach to the bend.

<b>ITEM 4.9</b>	<b>ROSEBERRY STREET, BALGOWLAH - SHARED PATH, RAISED PEDESTRIAN CROSSING AND REFUGE ISLAND</b>
<b>REPORTING OFFICER</b>	<b>ENGINEERING INTERN</b>
<b>TRIM FILE REF</b>	<b>2022/144558</b>
<b>ATTACHMENTS</b>	<b>1 Roseberry Street, Balgowlah - Plan 2 Roseberry Street, Balgowlah - Engagement Report</b>

**GEOCODES: -33.785492, 151.268225**

## **REPORT**

### **BACKGROUND**

Council's 'Move – Northern Beaches Transport Strategy' in 2019 identified that shared paths are a way to reduce reliance on vehicles and encourage active travel and healthy recreation in walking and cycling. Council proposes to extend the shared path network along Roseberry Street in line with Council's long-term vision for the Balgowlah and Manly Vale precincts.

### **LOCATION**

- Roseberry Street is a local road with a 50km/h speed limit.
- Roseberry Street has a road width of approximately 9.5m.
- Roseberry Street has an existing footpath with 1.2m width on the eastern side and a 1.5m footpath on the western side of the street.
- Roseberry Street, south of Kenneth Road is Balgowlah's industrial area, with supermarkets, fitness centres, shops and cafes.
- Roseberry Street, north of Kenneth Road is comprised of medium density housing.
- There is an existing shared path on Kenneth Road from Roseberry Street to Balgowlah Road.
- Manly West Park and Manly West Public School are located nearby to the proposed Shared Path.

### **ISSUES**

- Concerns were raised regarding safety on the footpath, with cyclists riding.
- Concerns were raised regarding safety at the driveways in the industrial area.
- Concerns were raised that crossing Kenneth Road is unsafe.
- Concerns were raised that Roseberry Street is too busy with vehicle traffic.

## PROPOSAL

Council has undertaken a review of the location and proposes to widen the existing path to create a new shared 2.5m wide concrete path that will extend from Koorala Street to Balgowlah Road to offer better connection to supermarkets, fitness centres and surrounding shops and cafes. This will create a safer footpath with more space for all users.

As part of this shared path project, the refuge island on Koorala Street is proposed to be upgraded to a raised pedestrian crossing with a 75mm high threshold. Additionally, on the eastern leg of the intersection of Kenneth Road and Roseberry Street, the splitter island is proposed to be upgraded to a refuge island with handrails. These will improve crossing opportunities on Koorala Street and Kenneth Road.

## PEDESTRIAN AND CYCLING IMPACT STATEMENT

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will improve cycling facilities and will connect to an existing shared path on Kenneth Road.
- The proposal aims to improve pedestrian facilities and walking paths.

## CONSULTATION

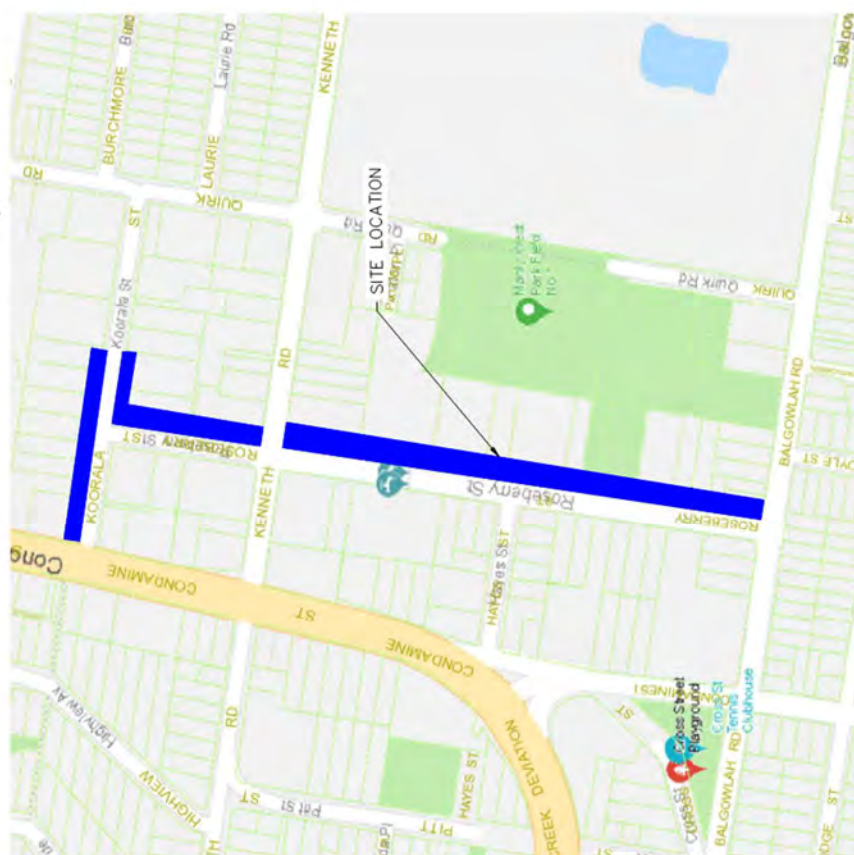
Consultation was conducted through Council's Your Say page with 150 responses. Letters were distributed to 400 homes near Roseberry Street, and a stakeholder engagement was conducted with 2 attendees. The full engagement report is seen as Attachment 2 – Engagement Report.

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## RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee supports the:

- A. Construction of a 2.5m shared path on the north side of Koorala Street from Condamine Street to Roseberry Street.
- B. Construction of a 75mm high raised pedestrian crossing on Koorala Street.
- C. Construction of a 2.5m shared path on the east side of Roseberry Street from Koorala Street to Balgowlah Road.
- D. Construction of a refuge island on the east leg of the intersection of Kenneth Road and Roseberry Street.



## LOCALITY PLAN

DRAWING NO.	DESCRIPTION
10001	COVER SHEET AND INDEX
1001	CIVIL WORKS PLAN SHEET 1
1002	CIVIL WORKS PLAN SHEET 2
1003	CIVIL WORKS PLAN SHEET 3
1004	CIVIL WORKS PLAN SHEET 4
1005	CIVIL WORKS PLAN SHEET 5
1006	CIVIL WORKS PLAN SHEET 6
1007	CIVIL WORKS PLAN SHEET 7
22001	LONGITUDINAL SECTION SHEET 1
22002	LONGITUDINAL SECTION SHEET 2
22003	LONGITUDINAL SECTION SHEET 3
22004	LONGITUDINAL SECTION SHEET 4
33001	SIGNAGE AND LINEMARKING PLAN
4001	STANDARD DETAILS SHEET 1
4002	STANDARD DETAILS SHEET 2
4003	STANDARD DETAILS SHEET 3
4004	STANDARD DETAILS SHEET 4
4005	STANDARD DETAILS SHEET 5
4006	STANDARD DETAILS SHEET 6
4007	STANDARD DETAILS SHEET 7

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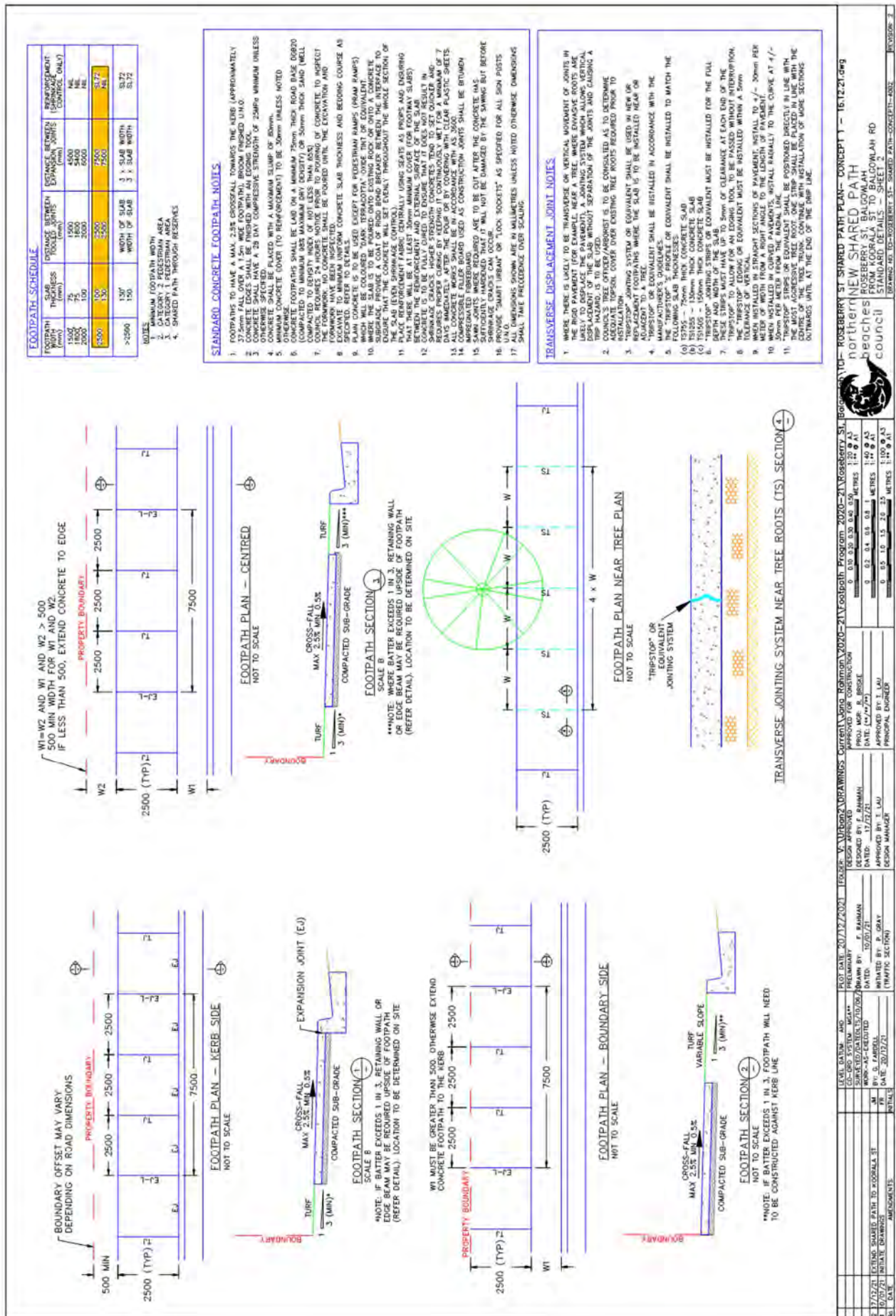






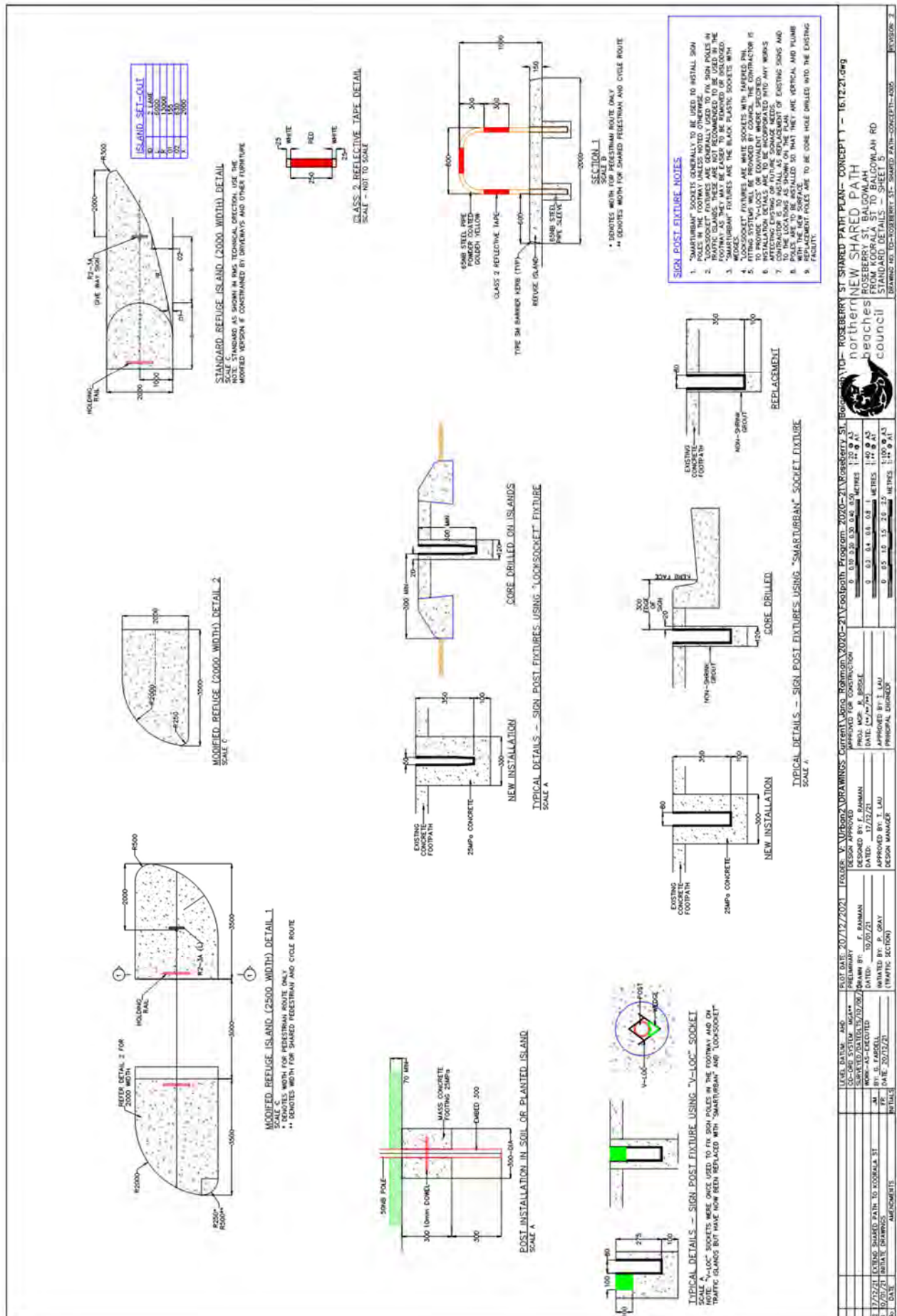


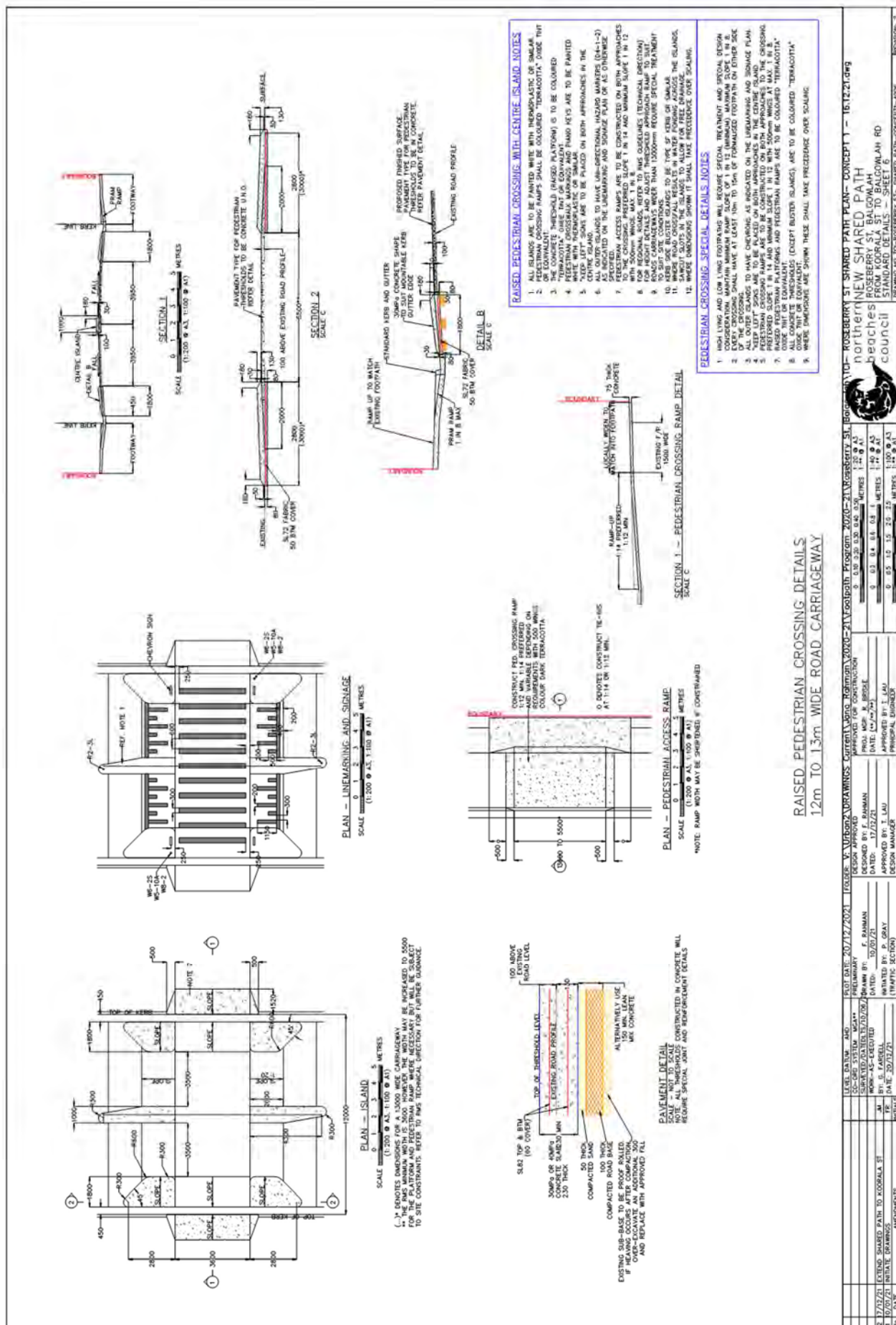
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## Community and Stakeholder Engagement Report

### Roseberry Street Shared Path

Impact level: Four

Consultation period: 27 April 2021 - 31 May 2021

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## 1. Summary

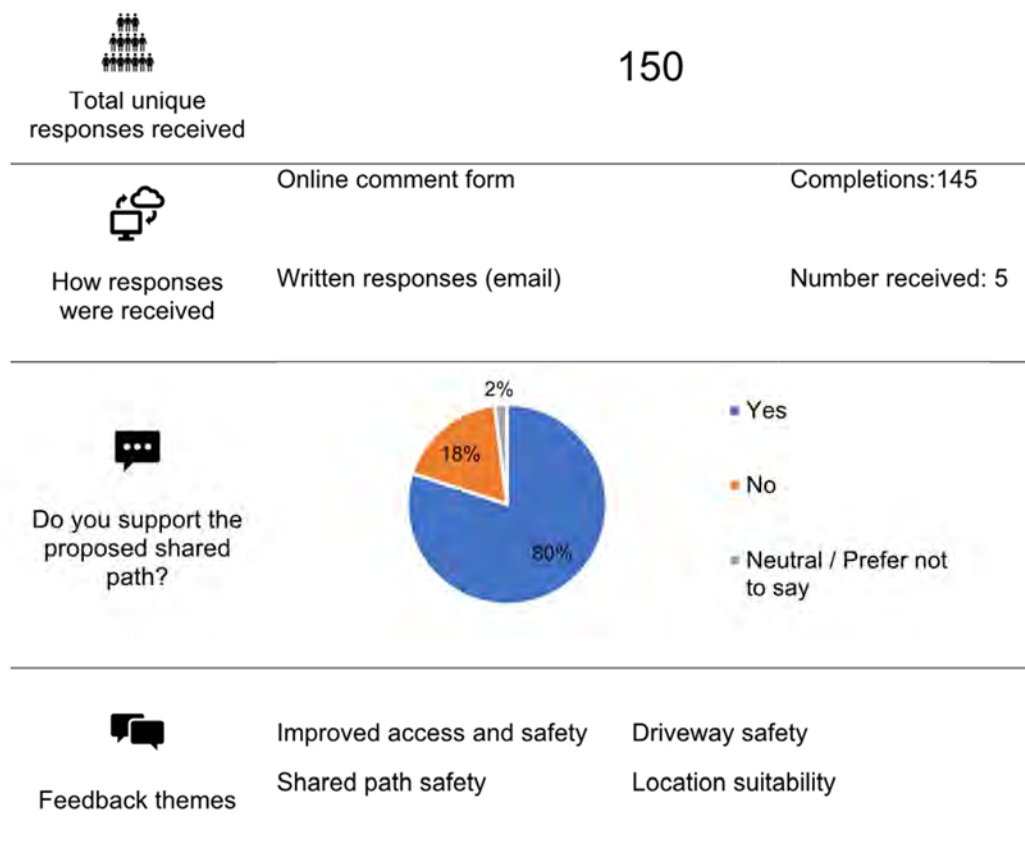
This report outlines the community and stakeholder engagement conducted as part of our proposal to upgrade the existing path along Roseberry Street conducted from 27 April 2021 to 31 May 2021.

The proposed establishment of a shared path along Roseberry Street, Balgowlah and Manly Vale aims to improve safety and connectivity for all path users, whilst also providing improved connection to supermarkets, fitness centres and surrounding shops and cafes.






Findings shows a high level of support for the proposed shared path along Roseberry Street with respondents citing the anticipated benefits such as improved access and increased safety.

Respondents who were not supportive of the proposal felt that shared paths were generally unsafe highlighting that they cause additional conflicts with . Concerns about the suitability of a shared path in this area, particularly relation to road safety and the interaction between path users and driveways were also raised.

### 1.1 Key outcomes



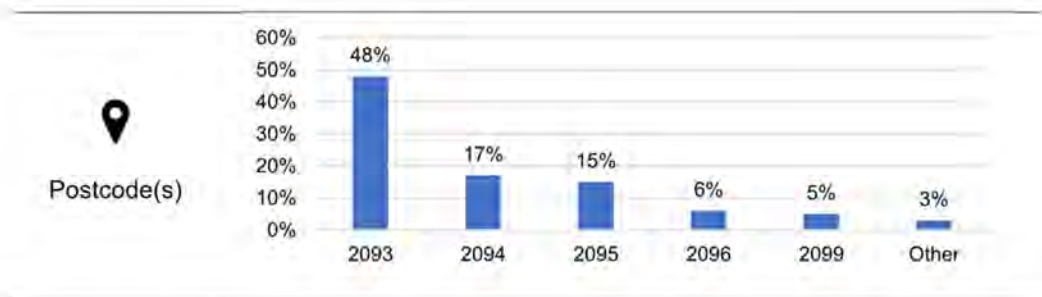
## 1.2 How we engaged

	Have Your Say: visitation stats	Visitors: 720	Visits: 860	Av. time onsite: 1m
	Print media and collateral	Letterbox drop: Postcode - 2093		
	Electronic direct mail (EDM)	Site signs:		Distribution: 400 homes
	Key stakeholder engagement	Community Engagement (fortnightly) newsletter: 3 editions Council (weekly) e-News: 1 edition		8 signs
	Meeting: 1	Attendance: 2 people		

## 1.3 Who responded<sup>1</sup>

	Gender	<ul style="list-style-type: none"> <li>Male</li> <li>Female</li> <li>Other id.</li> <li>N/A</li> </ul>	
	Age group(s)	<ul style="list-style-type: none"> <li>&lt;25 yrs</li> <li>26-50 yrs</li> <li>51-75 yrs</li> <li>76+ yrs</li> <li>N/A</li> </ul>	

<sup>1</sup> Demographic data was gathered by request only. The data represented only includes those respondents who provided this detail.



## 2. Background

Our Move – Northern Beaches Transport Strategy in 2019 identified how we can encourage active travel to help break the reliance on cars as the main way we travel across our area.

Shared paths are one way we can achieve this while also providing opportunities for both transport and recreational benefits and promoting a healthy lifestyle through active travel.

We proposed an extension of our shared path network along Roseberry Street in line with Council's long-term vision for the Balgowlah and Manly Vale precincts.

The proposal included widening of the existing path to create a new shared 2.5 metre wide concrete path that will extend from Koorala Street to Balgowlah Road to offer better connection to supermarkets, fitness centres and surrounding shops and cafes.

The community were presented with a proposed alignment plan along with artist impressions of what it could resemble once constructed.

The project's impact level four Community and Stakeholder Engagement Plan was devised on a single stage approach.

## 3. Engagement objectives

The key objective of the engagement was to identify community and stakeholder concerns and gather local knowledge and values.

We aimed to provide accessible information so community and stakeholders could participate in a meaningful way.

## 4. Engagement approach

Community and stakeholder engagement for the Roseberry Street Shared Path was conducted over a five week period, from 27 April 2021 to 31 May 2021.

The engagement was planned, implemented and reported in accordance with Council's [Community Engagement Matrix](#) (2017).

A project page was established on our have your say platform with information provided in an accessible and easy to read format.

The project was primarily promoted through our regular email newsletter (EDM) channels.

Feedback was captured through an online comment form embedded onto the have your say project page. The form included a question that directly asked respondents for their level of support on the proposal.

An open-field comments box provided community members a space to explain or elaborate on their support, not support or neutral sentiment as well as any other feedback they wished to contribute.

A meeting was held onsite to discuss the proposals at the request of adjacent property North Shore Cement and Sand.

## 5. Findings<sup>2</sup>

Feedback collected during consultation indicated a high level of support for the proposed shared path along Roseberry Street with respondents citing the anticipated benefits improved access and safety would have for people walking and riding bicycles.

The majority (80 percent) on responses were supportive of the proposed upgrades to the path in Addiscombe Road. Most agreed that the anticipated benefits to access and connectivity for people walking and riding bikes in the local area was beneficial to the local community.

Feedback received that was not supportive was primarily relating to safety. See Table 1 for a completed breakdown of the issues raised.

**Table 1: Issues raised in responses received (qualitative data)**

Theme	What we heard	Council's response
Safety (Path)	Concerns that shared paths are dangerous or uncomfortable for pedestrians due to shared use with people riding bicycles.	Transport for NSW Centre for Road Safety released a discussion paper on shared paths in 2015. The concluding remarks, based on evidence, stated that shared paths represent a relatively low safety risk.  Shared paths are designed to be as safe as possible with adequate width.
Safety (Driveways)	Concerns were raised that the shared path interacts with driveway crossings where cars and trucks) need to cross.	There is an existing footpath that currently caters to pedestrians. Vehicles must currently give way to pedestrians and path users. The proposal will make the path wider and more prominent which should make vehicles more aware of crossing that path.
Location	Concerns that Roseberry Street is a busy area for vehicles (including trucks). Some respondents commented that	We acknowledge the area is relatively busy. This proposed shared path aims to make the area more attractive for people to walk and ride a bicycle which may also help reduce traffic congestion.

<sup>2</sup> Note: This report represents what Council has heard as accurately and transparently as possible by using consistent quantitative and qualitative analysis techniques.

	pedestrians and bicycle riders should not be encouraged into this area as it is too dangerous.	The path offers alternative transport options for people to access business and shops in the area and provides a path for bicycle riders to use rather than being forced to share the road space with vehicles.
Road crossings	Concerns that crossing Kenneth Rd is dangerous.	The project will upgrade the crossing point to make it safer.  There is potential that this intersection may be signalised in the future which will further aid safer crossing.

During the consultation/exhibition. Council received a number of questions either through direct contact or within feedback received.

**Table 2:** Questions and Council's response

Question	Council's response
Where will the extra path width come from?	The existing footpath would be widened towards the kerb.
Can street lighting be upgraded?	This request is out of scope for this project. Manager, Transport Network has been informed of this request.
Will bicycle parking racks be included as part of the project?	Bicycle parking exists in multiple locations along Roseberry Street or in close proximity. For example, Outside Roseberry St Café, Aldi, Woolworths and Bunnings. Further bicycle parking may be added if a suitable location is identified. Council's Development Control Plan requires new developments to include bicycle parking.
Why would people ride their bikes in this area?  How will they carry their groceries, fridge, or lumber home on their bikes?	People are currently riding bikes in this area to access supermarkets and other businesses.  It is understood that a bicycle may not be practical for all shopping trips, however shared paths create a safer and more accessible connection for people to access whether they are walking, riding a bicycle, using a mobility scooter or pushing a pram.  The provision of shared paths and encouragement of active transport over car use is in line with Council's Move - Northern Beaches Transport Strategy.

## Appendix 1 Verbatim community and stakeholder responses\*

Number#	Comment/submission
1	We need a lot more shared paths and dedicated bikeways. The planned path needs to be wider to remove dangerous edges and allow enough room for users in opposing directions to pass. It also needs crossing at Kenneth Road. It needs to be linked to Manly West school. And the west side of Rosebery is a mess. It needs the same treatment to encourage cycling
2	There is no demand or need for this shared path. It would be better to sort out the traffic by making a Roseberry St one way and linking quirk road though as one way in the opposite direction. If anyone has done a study on this area this will make sense. Then at least we can get the traffic to 'move'. No one is cycling to the supermarket or gym, please just get the traffic moving.
3	The Northern Beaches is attempting to reduce the by 30% but the sales of electric vehicles is under 1% compared to the over 50% of New Cars in Norway being Electric Vehicles So adapting the Danish Bike Path Planning may help "the Northern Beaches Council achieve a 30% decline in GHG emissions, but providing a SAFE BIKE PATH solution for both kids and adults. The Manly Vale area is technically able to connect to Manly Beach within a flattish route.
4	Would be great for walkers and runners as the path is quite narrow, particularly around corners
5	Greater Manly Residents Forum, following discussions about the proposed shared path in Roseberry Street on 19 May 2021 voted to oppose this project at this time. Whilst the concept is a good one, executing a shared path in this area at this time will no doubt result in serious accidents involving pedestrians, cyclists and vehicles. Roseberry Street's extended peak times each day make it extremely busy. The combination of double -b trucks, semi-trailers, vans, and many commercial and private vehicles are drawn to the area by Woolworths, Aldi, Bunnings, Harvey Norman, Northshore Sand and Cement., furniture shops including SmithMade and Fantastic Furniture, Bing Lee, as well as Manly Freezers, 4 Paws Veterinary Clinic, F45 gym and various other business enterprises. All these vehicles vie for space on the road and with the massive number of delivery trucks it is a normal occurrence to find trucks double parked in Roseberry Street waiting their turn to enter a premises, causing other vehicles to drive around them on the wrong side of the road. It is the norm to find double-b semi-trailers reversing into or out of Northshore Sand and Cement with associated workers using hand signals to hold traffic for up to 7 minutes whilst they enter/exit premises. Walking or cycling along Roseberry Street between Balgowlah Road and Kenneth Road necessitates negotiating many very busy business driveways. It is not unusual to find traffic banked up around the roundabouts at Roseberry Street and Kenneth Road and Quirk Road and Kenneth Road. The entire area bounded by Kenneth Road, Condamine Street, Balgowlah Road and Roseberry Street is an extremely busy retail/light industry area which will be even more heavily impacted with the advent of the Northern Beaches Tunnel and the implementation of the Local Housing Strategy in the area. Putting in place a shared walkway in this area at this time is icing the cake before it is

\* Personal details have been redacted where possible. Spelling and grammatical errors have been amended only where misinterpretation or offence may be caused.

132 online responses provided feedback in addition to the mandatory sentiment question.

	<p>even baked. GMRF asked the council to put this proposal on hold until the entire area is reassessed for future traffic needs and impacts.</p>
6	<p>How about more eating area outside the sandwich shop next to Matt Black Like in manly and what they are planning for DeeWhy</p>
7	<p>this is one of the most dangerous streets in the area. most traffic is trucks. the grass berms need to be removed to make driving in Roseberry street safer for all concerned. as for even considering pushing a baby along there is so dangerous as is biking This is an Industrial area Not recreational. look at the next street where the small park is !!</p>
8	<p>I regularly ride my bicycle to Aldi and/or Woolies in Roseberry St. I'm a pretty confident cyclist, but it sure would be nice to be off the shared road for the segment of the journey from Kenneth to the shops.</p>
9	<p>But the path is on the wrong side of the road.</p>
10	<p>This is a great improvement to Manly Vale Balgowlah and will help greatly - particularly since the opening of Aldi - the pavement on the Eastern side is currently too narrow and very much in need of repair.</p>
11	<p>This will encourage more foot and bike traffic. We need bicycle parking!!!</p>
12	<p>Rosebery street is an extremely congested, dangerous street with traffic and large trucks servicing these large stores and supermarkets - it is NOT suitable to encourage PEDESTRIAN TRAFFIC - especially children- who have a liable to be the casualties- of the road traffic.</p>
13	<p>A pedestrian crossing in the middle of the southern block of Roseberry Rd would be great. It's hard for people to cross over. The bike path is on the east side &amp; Most of the useful shops are on the west.</p> <p>Thx for giving bike riders space.</p>
14	<p>My workplace is in this area and I would love to see an extended shared walk way in this area. Improved lighting and path side seating would be wonderful for us workers. I am also interested in setting up an Artisans Market and wondering whom I contact to inquire about this...</p>
15	<p>better than being run over by cars and trucks.</p>
16	<p>Have previously submitted feedback about the missing pavement outside 20 Roseberry Street, which (ironically) is normally parked across by a Northern Beaches Council ute.</p>
17	<p>THE STREET HAS MANY BUSY DRIVEWAYS COMBINED WITH HEAVY TRAFFIC ALONG ITS LENGTH WOULD MAKE IT POTENTIALLY DANGEROUS FOR CYCLISTS. THE NORTHERN END OF ROSEB IS USED AS A SHORTCUT FOR MORNING AND AFTERNOON TRAFFIC. THE ROSEB/KENNETH ROAD ROUNDABOUT IS NARROW AND INSANELY BUSY AM /SCHOOL PICKUP/ AND PM. CROSSING KEN RD AT ANY POINT IS DANGEROUS DUE TO THE FAST MOVING TRAFFIC FLOW. THE QUIRK/KENETH RDS ROUNDABOUT IS ALMOST AS DANGEROUS AS ROSEB. INSTALL TRAFFIC LIGHTS AT EITHER ONE OFR BOTH ROUNDABOUTS WOULD SOLVE THE PROBLEM.</p>
18	<p>Excellent improvement to semi-existent bike and pedestrian path. Enormous improvement for cyclists in this busy (and at times exceptionally busy) road providing a means for cyclists to leave the road for the many cars and trucks using this thoroughfare. The nature of Roseberry Road has changed in recent times and making the pathways</p>

	safer and more attractive to pedestrians and cyclists will result in more local people accessing the numerous retail outlets without having to access the area by car. What a win for everyone !
19	Guys, Please open up Quirk Road! You are shuffling the deck chairs on the Titanic stuffing around with this - fine to do a cycle path but what's the point? Roseberry Street is a jammed nightmare after all the development. The lights at Condamine are overcrowded, Balgowlah Rd tailbacks go hundreds of metres back at the junction of Kenneth Rd. See attached. Do some updated research. You need more flow from Balgowlah / Fairlight up to North Manly / Freshwater. Right now at many times of the day it is way too difficult. Quirk Rd is sitting there as a solution to let traffic flow south to north in this area. Please open it up and let us breathe!
20	For ease of traffic flow and ease of viewing pedestrians particularly when they cross the road, could we remove all on street parking on Roseberry St between Kenneth and Balgowlah Rd. There is plenty of off street parking - Woolworth's; Bunnings; Harvey Norman, Fantastic Furniture etc.
21	It sounds great along the business section but as a resident between Kenneth road and koorala (on the corner 70 Kenneth road) I am concerned about bikes flying past our place when my 16month old son is playing on the grass out the front... especially right on the corner. I'd ask you to consider shortening the path to not include that small stretch between Kenneth and koorala
22	Long overdue.
23	This will lead to increased safety for walkers and cyclists.
24	This really is the most outrageous suggestion. As a cyclist there are already adequate provisions made in that area & no cyclist in their right mind would cycle down Roseberry Street. (I am a cyclist).  Roseberry St is a disaster waiting to happen. It's usage has changed considerably over the past 15 years with way more industrial type retail outlets, as well as the B-Line car park right down the other end of the road. This has resulted in a massively increased usage by cars & trucks to places such as Bunnings, Aldi, Woolworths, Harvey Norman as well as the businesses set up in the small industrial parks scattered down the road. The road cannot cope with the change in usage as it is and this is NOT an appropriate option for the street. Unfortunately Council has not been on the front foot in anticipating the current issues once the change of retail mix started being approved. The parking in front of the shops is also a disaster waiting to happen.
25	Great idea and design
26	I think a shared path is much needed down Roseberry street, and it is important that it is unobstructed with telephone poles, and other obstacles to allow for safe passing given the volume of pedestrians and children on bicycles in the area. It also needs to be well marked for the drivers pulling out across the path.  Thanks heaps for kicking this one off!
27	Awesome idea! Will make the street a lot safer
28	A crossing at Kenneth Rd as part of the path is required to make the path functional and foot traffic to flow on the path.
29	We should also be improving the roundabout at the other end of Roseberry St

30	Yes support it 100%. Really good idea. It should be on both sides of the street. Put a proper crossing at the Balgowlah Rd intersection. Improve the roundabout as well.
31	some very poor sections of path currently, with high usage for families which find it difficult and slightly unsafe, s new wide path would be great.
32	This is one of the worst ideas that I have ever seen from the council. Have you thought of all the safety aspects of mixing, Children on bikes, many driveways, an already congested street, and an area with a street that ends up in a major street north and south. Why would people ride their bikes in this area? How will they carry their groceries, fridge, or lumber home on their bikes? Where are all the bikes going to park if they stop for a coffee or imagine them riding into Bunning's car park or even Woolworths? Some of these might seem lame but this project has not been thought through. The street is always busy and is often clogged with traffic. There is not enough parking in the street and area as it is, except when noncustomers use other businesses car parking facilities. The bike path does not lead onto any other major bike path. Bike riders already have a north-south shared path at Quirk Street. Do a risk assessment and think of the frustration of all the pedestrians and motorists that use this area.
33	It looks good and connects three supermarkets but why not connect further to Manly West primary and Mackellar girls High schools
34	Safer crossing of Kenneth Road is desperately overdue.
35	Hi Roseberry Street would be the busiest street in Balgowlah on any day and more so at weekends. I am very concerned about the access to businesses that are along this street. Council trucks, trucks for Harvey Norman and Bunnings and Woolies all use this street to access the business. There are also people carrying groceries on the pavement, you have trees blocking the view from driveways to pull out onto Roseberry street making a quick access important. At the corner of Roseberry and Balgowlah Rd the shops have parking that makes it very congested and difficult. A bike path there where the customers are going into the shops from their cars and a bike path could be dangerous.
36	The proposed upgrade will improve pedestrian movement.
37	I enjoy cycling around the Northern Beaches. The more shared paths and cycling paths the better.
38	It will make that area safer for pedestrians
39	Because this street has such an high traffic flow with trucks & cars I feel it unwise to increase foot traffic which could be dangerous to pedestrians. If at all possible I would prefer pedestrians and vehicles to be kept on completely separate routes.
40	It is a dangerous street, with large trucks and vans entering and departing several properties resulting in extremely poor traffic flow. If a wider footpath reduces the width of the road, traffic flow would become worse I would only support a wider footpath if parking on Roseberry Street is limited to one side of the street.
41	The path is needed to keep cyclists safe along a road where traffic is often chaotic.
42	Excellent Idea Well Done
43	I dislike these shared paths. As a pedestrian I find them rather nerve wracking and the bicycle users are usually going very fast and expect you to get out of their way. These paths are now seen as cycleways with the occasional use by pedestrians.
44	Will make it safer and easier to walk around in a very congested street.

45	Roseberry St is already extremely busy and it seems that a shared path would restrict traffic and narrow the road even more. Is it intended to make roseberry st one way. If so I would support this if Quirk Rd became the alternate by opening it up.
46	That area is very congested for bikes in relation to car traffic, so I think it's a good idea. I don't see a lot of pedestrians using that area much.
47	Any shared path facilities in this neighbourhood are welcome given due to the busy nature of the surrounding streets. Pedestrians and cyclists definitely feel unprotected without something like this in place.
48	This sounds great - but until we work out a way to stop cars from swinging across the pavement/shared walks at speed into driveways, we are only increasing the chances of more accidents. Roseberry Street is a nightmare to walk down because of the number of driveways/access points/parking lots, and the fact that drivers seem to think they have right of way even across pedestrian paths. It is worse on Roseberry street because of the numbers of large vehicles often parked along the streets, or in driveways, etc., so cars don't see pedestrians. Council should have thought through a better plan for Roseberry Street a long time ago....
49	The more shared paths the better. It's too dangerous riding a bike in the road.
50	This road is far too busy for a shared footpath. There are so many businesses and retailers between Kenneth & Balgowlah Rds that have driveways used by B double trucks, garbage trucks, Hop Skip & Jump buses, council vehicles, couriers, tradies and the general public. This intense use needs to be monitored very carefully by pedestrians and so to add bike riders to their concerns will inevitably lead to accidents.
51	Shared paths with cyclists are a menace and a danger to pedestrians. Cyclists mostly do not ring a bell when nearing pedestrians and many travel at speeds which are inconsiderate and too fast.
52	More attention to the area towards Balgowlah Rd./Rosebery St. Parking is dangerous. Walking/riding and driving by the shops is very dangerous. Footpath needs to be improved and pedestrian crossing at Kenneth road as well
53	This is a great proposal. The current footpath has very light pedestrian traffic. It would provide safe access to Aldi, Woolworths, F45 and other businesses along that route. A very practical proposal
54	Safer and easier to negotiate than what is there at the moment.
55	Great Idea
56	The attached image from City of Sydney shows how Roseberry Street should look, with separated footpath, cycle lane and road. The plan should also include additional planting/trees to make up for the extra hard surfaces being laid to help decrease heat in summer. Northern Beaches Council's obsession taking the easy way out and building shared paths will look very short sighted in years to come. With the amount of units being built along Condamine Street over the past 5 years, there must be enough developer contributions available to the area to do this scheme properly and turn Roseberry Street into a genuine local high street worth visiting.
57	Will there be bike racks in numerous locations down Roseberry Street? I'm guessing there'll need to be at least 50 along the street.  There's many driveways down this area that cross the proposed path - will the shared path when crossing these driveways be coloured green to indicate to drivers than pedestrians and cyclists have the right of way?
58	As long you don't restrict the dam bicycles from the normal footpath I am not supporting spending millions for bike rides, which breach the cycle rules and there is nothing done for the safety of pedestrians!  Please informed bicycle riders what the rules are, as most of them just know wrongly

	that they can use a normal footpath! Or have to walk over a pedestrian crossing. It is time otherwise I will lodge a legal courtcase!
59	People will continue to cycle along Roseberry Street regardless of whether this path is constructed or not. The path however would be the responsible choice for Council given that the number of people cycling is increasing all the time. It would be safer for the cyclists and reduce the current traffic bottlenecks along this street during peak hours.
60	Great idea. It's dangerous right now and would provide more options for the people keen to ride their bikes rather use their cars. Roseberry st is often choked with cars. I work in the street and would love my kids to ride to my work from our home in Fairlight but currently it's not safe. More wide bike paths in the area would be welcome by our family.
61	As a bike rider with kids this makes going to the supermarket so much safer to avoid the many fast cars and trucks
62	Suggest extend path along Kenneth Rd southside to Quirk Road providing a safe connection to the existing Kenneth Rd path towards Manly Aquatic Centre.
63	As long as you don't narrow the road, I'm supportive. The traffic on this road is already very heavy. It was totally untrue that it could handle another supermarket (like Aldi).
64	It is currently used for both bikes and pedestrians. Making this formal will mean less contention between the different users.
65	I strongly support this idea. As a resident in Manly Vale I often avoid cycling to this area as the road is so busy. The footpath is underutilised and I believe this would make it much more user friendly space.
66	I use a pram and would greatly appreciate this
67	Great
68	Good for safety of all pedestrians . Keep up the good work
69	I am supportive in principle of cycling infrastructure, it I am hesitant about this one because the linkages to the other path at the northern end would entail crossing a extremely busy intersection, and the crossing at the Aldi driveway is also likely to invite cyclist-car conflict. I am also not sure of the objective as there is a parallel link in a street to the east between Kenneth road and Balgowlah rd, so I would be interested to know what the extra path is aimed at achieving. The continued preferencing of share paths over separated cycle ways and use of separated paths that do not cross roads is concerning as the latter are safer and more likely to be used by a wider range of the population.
70	Great idea safer for pedestrians. Concerned about the trucks exiting from the Northshore Sand and Cement. Would like to see better lighting at the new pedestrian crossing.
71	I'm concerned about the business of Rosebery st, poor visibility of vehicles turning into driveways and traffic congestion. It's a safety and traffic concern and the whole road needs a review.
72	Wider and safer path for walkers shoppers cyclists absolutely necessary. Entrances and exits to major commercial properties made more visually aware
73	This will definitely improve Rosebery Street for pedestrians and cyclists. Please make sure that the shared path is continuous across driveways and side streets so that it is clear that shared path users have priority and vehicles have to slow right down to turn.
74	Not sure about this, Roseberry street is a very busy road, servicing three supermarkets, vehicles will be forced into a narrow corridor. Will there also be a reduction in speed to less than the current 50 kph?
75	My office premise is on Balgowlah Road Balgowlah and over the last 21 years since we have owned the building we have seen a very significant rise in vehicular traffic, and in particular since Woolworths, Harvey Norman, Aldi (recent) and Bunnings have

	<p>produced a very significant increase in light and heavy commercial vehicles, from Tradesman's Utes, pantech trucks to very large semi trailers. Many of which sit across the footpaths to unload (Harvey Norman trucks in the main) and so having a formal bike shared space is inviting the Council into a very risky situation with litigation should there be an accident which can be attributed to the council's decision to undertake this proposal.</p> <p>I see no evidence where there is a business case for this. Bicycle traffic studies being one element which would assist to making a considered decision.</p> <p>Weekend bicycle traffic might be a little more, but mainly on Balgowlah Road.</p>
76	<p>Terrific concept to enhance safety for all. It would also be advantageous to create pedestrian paths to Manly Vale Public School.</p> <p>For example:</p> <p>Extending the pedestrian path on the eastern side of Kenneth Road to Mildred Avenue and Link Street heading to the school. In addition, constructing a pedestrian path on The eastern side of Horning Parade and on Highview Avenue. Improving access to path/stairs on the eastern side of Highview Avenue linking to Condamine street lane. I understand there is council land on Horning Parade that could be linked to Highview Avenue as a stairway.</p>
77	<p>This Rd has many driveways used by large trucks. Especially North Shore sand cement as an example. I don't believe this is a safe idea. Also there is already a bike path running along Manly West park so don't see the necessity. Surely the money for this could be better spent.</p>
78	<p>Crossing the road at Kenneth Road remains a big issue in all directions.</p>
79	<p>Fully support this.</p> <ol style="list-style-type: none"> <li>1. Please consider how to manage the vehicles that park directly in front of 20 Rosebery St (which is the property immediately south of Aldi) - as these vehicles typically park across the path, causing inconvenience and safety issues. I suggest using bollards to prevent this. Important matter to address as this is a particularly high activity spot adjacent to the two supermarkets.</li> <li>2. The shared path should have priority over vehicles entering/exiting driveways (i.e. cyclists should not have to give way to entering/exiting vehicles).</li> </ol>
80	<p>Rosebery St is one of the most dangerous streets on the northern beaches. The traffic especially on weekends is atrocious, often banked up to Condamine St and onto Kitchener St. Pedestrians, bikes, cars and many trucks for delivering to multiple businesses make it a heavily congested area.</p> <p>The roundabout at Balgowlah Rd needs upgrading. The pedestrian crossing on that corner is very dangerous and should be moved away from the roundabout further down Rosebery St.</p> <p>Cars parked at Rosebery St shops near the roundabout reversing out and often cause traffic jams and together with car / truck traffic at Manly Freezers is a death trap waiting to happen.</p> <p>Cars shouldn't be parking on any side of Rosebery St. It's far too dangerous. Cars must not be parked in any of part of Rosebery St between Balgowlah Rd and Kenneth Road unless Council widens Rosebery St. Often I drive down Rosebery St with trucks illegally parked on both sides of Rosebery St. This is not being policed and makes it very difficult to pass.</p> <p>There have been numerous developments in Rosebery St over the last 10 years. Bunnings, Aldi, Woolworths and many more buildings, Yet Council has not used any money to improve Rosebery St. This is a disgrace. Council receives contributions from</p>

	<p>developers yet no monies have been used in any way to improve safety on Rosebery St !!!</p> <p>Rather than just creating a shared walkway for pedestrians, Council must come up with better strategies to improve car/ traffic in conjunction with pedestrian safety. Not just implement shared pathways merely to tick a box.</p>
81	<p>Great idea. Might stop people driving between shops and entice them to walk in order to shop at multiple locations along Rosebery Street.</p>
82	<p>I think there is need for better street infrastructure for the growing community in the Manly Vale/Balgowlah area.</p> <p>The current footpath is not really wide enough for prams, children on bikes or the elderly on wheelchairs/frames. Traffic congestion is also a growing issue along Roseberry Street, so the widened paths means a safer area with less exposure to the road for pedestrians.</p> <p>It would also be nice to keep growing the greenery in this area, to the idea of trees and planter boxes along the road is a fabulous idea to lift the area out of a strictly industrial landscape.</p>
83	<p>Has to happen. The footpath is wide enough, traffic chaotic and congested. Currently unsafe for bikes and pedestrians. Also include a sign at pedestrian crossings telling bike riders to dismount. Illegal and dangerous.</p>
84	<p>Good to see some infrastructure for cycling. The road is currently challenging to ride on. I would prefer a dedicated cycle way, as negotiating the path with pedestrians and prams will also be dangerous.</p>
85	<p>It would be a welcome improvement on that side of the road as it is a tired and somewhat unkempt section of footpath.</p> <p>As well, lighting in the street is required, especially walking from the B1 Bus Stop and along Roseberry Street at night. While on the topic of lighting, Hill Street (at Balgowlah Road end) is bereft of lighting altogether. If attempting to walk from B1 Bus Stop (Condamine Street) to Sydney Road, to get transport to Manly, it is a bit scary as there is only one light near the Manly West Public School.</p>
86	<p>Widen the footpath if you wish BUT more importantly, concentrate on the road traffic situation and give some consideration to motorists for a change. You have ruined Manly for the people who live and shop there and you have plans to do the same in Dee Why. You are prioritising people on bikes (the rudest, most self centred group), and pedestrians who NEVER EVEN LOOK before they step onto a pedestrian crossing because they are totally concentrating on their phones. Try spending some money on education for those groups.</p>
87	<p>I think it is a good idea to improve the foot path but I am not comfortable sharing paths with bikes as I find them intimidating. They often speed and don't ring their bells. Maybe there can be more controls regarding bikes</p>
88	<p>Shared path is a good idea, but the biggest issue on Rosberry St is the parking of trucks (both North Shore Sand &amp; Cement &amp; Harvey Norman deliveries) in the no stopping zones.</p>
89	<p>This path is important for kids to cycle down. It is dangerous other.</p>
90	<p>Yes, shared path is a good idea. Important to put extra safety measures in place to stop cars/trucks dashing out of driveways in front of bikes &amp; scooters. Eg. wide yellow lines painted at driveway exits to remind drivers to stop &amp; check for cyclists &amp; pedestrians. Also do this on other side of road particularly outside Bunnings where many drivers don't seem pedestrian aware.</p> <p>Locate signage for cyclist low height or paint on path as they don't gaze upwards.</p> <p>Improvements to crossing at roundabout are much needed. It scares me to see</p>

	<p>schoolkids trying to cross there.</p> <p>Extra bike racks outside popular businesses. Bike racks at Woolworths badly located as you have to cycle through carpark.</p> <p>Extend bike path to Mackellar Girls High &amp; Passmore Reserve.</p> <p>Maintain existing cycle track through Quirk Road. It's very good as no worries with driveway exits or trucks.</p>
91	<p>I am all for shared paths and more trees along the side of roads. However there are far too many butchered trees in the district due to their interference with overhead powerlines. Please consider whatever mechanisms local council have to enforce powerlines underground and out of site so that trees can grow into their natural shapes and enhance not hinder the visual landscape.</p>
92	<p>However - with the boom in e-bikes travelling at 25kph plus, 'Shared Paths' are not safe for pedestrians. There should be a dedicated cycle lane on this road as there's no reason to maintain street parking in a street with excessive off street parking (B-Line car park/ Bunnings/ Woolworths/ Aldi/ Harvey Norman/ Fantastic Furniture etc).</p>
93	<p>this street is so dangerous so this is welcome. i use this with my bike all the time as i shop and train there and do not have a car. it is by far very dangerous and unwelcoming most times. i also would like to add the parking outside the rosebery street cafe should not be the way it is. that is a congestive obstruction and also a super danger to all users and drivers and people.</p>
94	<p>I am supportive of more shared paths - they provide an opportunity for those less confident to bike with cars around, and make it easier for those driving cars too.</p>
95	<p>How about widening Roseberry St to 2 lanes each way. There is plenty of space and traffic hold up can be extreme at some times, particularly around the southern end with cars backing out of the front-in parking. It sometimes affects traffic on Balgowlah Rd.</p>
96	<p>Only if the Roseberry st is not made narrower. as this street with a roundabout at each end , is such a bottle neck, any time if the day.. We use Balgowlah rd and Kenneth st regularly.. We also shop in the Roseberry st precinct, The parking opposite Manly Freezers in Roseberry st is quite dangerous, with lots of trucks coming and going .</p>
97	<p>fantastic!</p>
98	<p>Better still would be to have a separate pedestrian and cycle ways</p>
99	<p>So happy Council is proposing this path!!! thank you. Currently, it is so dangerous trying to ride down this street, so most of the time, I just ride on the footpath which is too narrow and in poor condition. Thank you for making our neighbourhood more bike safe</p>
100	<p>The COVID era has seen many hundred more bicycles in use. Not all recent users are road savvy/aware so any opportunity to create shared paths to facilitate a whole range of people riding and walking safely should be encouraged.</p>
101	<p>It is essential as many shared paths are built for the benefit and safety of those not wanting to or unable to use a car.</p>
102	<p>As a parent with a toddler living in nearby Kenneth Road, this shared path extension on a busy Roseberry St thoroughfare is very much supported and welcome! Great idea!</p>
103	<p>Sound fantastic thank you</p>
104	<p>Connects key areas via active transport</p>
105	<p>This is fantastic. On road cycling on Roseberry is a bit of a nightmare as lots of merging traffic from supermarkets etc. Footpath is also low quality (especially around ALDI where it's gravel in places) so this will make a huge difference!</p>

106	Absolutely support this; please consider the safety of crossing for pedestrians across Kenneth rd section. As it is currently is a dangerous spot -
107	Great Idea The path near Manly freezers is too narrow, lots of mums and dads struggle with prams etc, especially when the hedge has overgrown
108	I just think it will make entering and exiting 50 Roseberry Street parking more hazardous as it's already a busy road. Good that you are keeping the existing trees though. Please don't get rid of those.
109	Absolutely - I regularly walk to Aldi with a pram from Balgowlah and the footpath is terrible and uneven! Definitely needs an upgrade.
110	Yes this is great but please please please please install more pedestrian crossings as part of the project. This area is becoming busier and it is becoming increasingly unsafe to cross Roseberry Street.  Crossings are needed at the very least around the roundabout at the intersection of Kenneth Road and Roseberry Street and across to Coles at Koorala Street.  Thank you and regards, [REDACTED]
111	How about to build a proper crossing for pedestrians around roundabout on Kenneth road and Roseberry street. People are speeding and driving like a crazy to get to the lights on green light. It's very dangerous to cross there!
112	I visit this area on foot nearly every day. There are very few cyclists but many pedestrians often with lots of shopping bags crossing between Aldi and Woolies. It could be quite dangerous crossing the cycle section as riders often travel at high speed and without care as in other similar shared path areas eg Balgowlah Road. Also there will be a sacrifice of grass and vegetation for yet more concrete. It is an unnecessary change in my view.
113	Crossing at the roundabout at the intersection of Kenneth & Roseberry St Manly Vale is extremely dangerous. And encouraging more foot/bike traffic will endanger more lives.  Cars are extremely impatient during peak periods and I have witnessed people driving over the roundabout, driving into the lane heading in the opposite direction in an attempt to get around cars that are trying to enter Kenneth Rd from south side of Roseberry St.  Having a shared path on Roseberry St between Koorala and Kenneth seems pointless as it goes nowhere. Having one down Quirk Rd on the eastern side makes more sense based on the bike and human traffic I witness.
114	I think the other side of the road needs the work and the angled parking needs amending urgently.
115	I'm always in favour of better routes for cyclists. We don't have a car, and some parts are dangerous or unsuitable for bicycles; especially when transporting kids. Also, please take bikes with trailers or boxes in front (bakfiets) into consideration when planning. Thank you!
116	Very belated after thought and window dressing. All those businesses many with their respective car parks should never have been allowed. It is packed with cars and often the roundabouts are blocked due to volume of traffic. The flow of traffic is often blocked as people wait to turn into car parks etc. It is not pedestrian or people friendly area and the foot path is not going to change that

117	I work around here and it could be a lot nicer. A new widened path would really improve not just access, safety and the ability to connect biking areas - but it would uplift the street which is very busy and starting to get a bit run down.
118	Please build this ASAP. The amount of traffic on Roseberry St makes it dangerous to ride.
119	Does this mean the 90deg parking area goes, that would be good, but then there is parking going to be provided. Surely not Bunnings and Harvey Norman .
120	Must do something about roundabout for pedestrians, prams and cyclists crossing on Kenneth rd and Roseberry street In all directions.
121	It's time👏👏👏 And we are soooooo grateful for the Woolworths/ALDI crossing. Many thanks to Council for saving our lives 👍
122	<p>I drive a car, I cycle and I walk - and the more room there is for all of us, the better. I feel that cyclists are particularly badly served in Sydney and that they, more than anyone, need better facilities. The fact that increasing numbers are riding E bikes which are heavier and faster and often ridden by older people makes that more important than ever. It has been estimated that in three years time, the sales of E bikes will outstrip those of conventional bikes and I've seen nothing by any council to take that into account when planning for the future. In fact the reverse seems to be happening - it is now impossible to cycle from Manly to Warringah Mall, for instance. Beyond ridiculous and demonstrates a poor lack of foresight. Please don't say there is no need for a cycle path because no-one cycles to Warringah Mall. We have all stopped because it is now too dangerous or, if you use the lift to cross the road, time consuming.</p> <p>I would also like to see signage along all bike paths changed to align with that in cycling-friendly Canberra. There, on shared paths, warning signs are directed equally at pedestrians as well as cyclists reminding them to stay on the left and keep dogs under control. On the N Beaches there are an absurd number of signs ordering cyclists to ring their bell with the result that cyclists are regularly shouted at to ring their bells when there is clearly no need.</p> <p>I'd also like us to follow Canberra in saying that adult cyclists on shared cycle ways do not need to wear helmets. As a former competitive cyclists, I would agree that cycling is a dangerous sport (although when I started racing, even Tour de France riders didn't have to wear helmets). However, it is a very safe form of transport. We need to promote it as such.</p>
123	<p>nice idea, but from where does the extra width that the proposal will need arise ?</p> <p>roseberry street is already a major congestion zone, that will not become any less busy in future</p> <p>the pedestrian crossing adjoining balgowlah road at the roundabout, and the perpendicular parking albeit well established by existing businesses, are clumsy, obstructive, and inappropriate for the situation</p> <p>some years ago i suggested that the power lines on the east side of the road were buried when the initial redevelopment of the street was undertaken - this would immediately widen the available pedestrian area and / or allow for a green barrier between cars and pedestrian traffic</p> <p>the concept image arrangements surely misrepresent the available space, unless you intend to narrow the already very limited width of what is a two way major traffic conduit, or somehow intend to make the traffic one way only</p>

	<p>short of redeveloping quirk road for cars to traverse burnt creek across manly west park, roseberry street will remain a major pinch point for any traffic between balgowlah and kenneth roads - there are no other conduits</p> <p>i'm keen to see the concept as a technical drawing rather than an artist's fantasy</p>
124	<p>Yes I think that a wider footpath is a great idea! But ! At Manly Freezers, in particular, has a dreadful time reversing into their loading dock! With parked cars opposite , that particular point it is a very dangerous spot for pedestrians! Cars bank up and then you will have people on the roads! It's just Not a safe combination!</p> <p>Also "brookvale sand " on the same side with large trucks , it would be an area I'd avoid with encouraging kids, prams etc! Maybe stop the wider path at Kenneth road!</p> <p>I think a planner / councillor should sit in a cafe on Rosebery street near Balgowlah road and observe! You'll be shocked by the traffic in Rosebery st! Cars/ trucks/ etc!</p> <p>It's definitely an Industrial area and not residential ! All reversing!</p>
125	<p>What a misleading concept image - road traffic is always gridlocked</p>
126	<p>An excellent proposal and fully supported. The new pathway is needed and would better utilise the current messy footpath arrangement on the Eastern side of Roseberry st. It would mean less cars in this busy area.</p>
127	<p>There is a serious traffic problem in Roseberry Street mostly caused by the continuous presence of very large delivery vehicles. For this reason it is imperative to open up Quirk Road to through traffic and restrict Roseberry Street to one direction traffic flow.</p> <p>Regards</p>
128	<p>The bike path without effective and safe traffic control at the Roseberry Street and Kenneth Road roundabout, is delivering infrastructure that does not provide safe passage across roads for those that use it - making it pointless.</p> <p>The road is extremely busy, cars do not slow down, they drive on the wrong side of the road to get around traffic (including over the roadabout), adding a pathway that will encourage use by pedestrians and cyclists to try and cross this busy and dangerous intersection - so traffic control is absolutely required.</p> <p>Traffic lights at this intersection is the best option for the level of traffic that goes through it now.</p> <p>The Roseberry Street and Kenneth Road intersection traffic can be reduced by connecting Quirk Road between Kenneth and Balgowlah Road.</p>
129	<p>I live nearby and walk along that existing path several times a week because of the supermarkets, VET and other retail furnishing businesses. It's crowded, with cyclists, pedestrians and dog walkers. a shared path would mean that it's safe to even push a pram or for an elderly person, to use their walkers. I can't wait for this to be built.</p>
130	<p>I live, exercise, shop and work in this area, Walking every where at different times of day I have never had a issues with the foot paths. Save the money and fix up the traffic issues</p>
131	<p>great initiative, lots of young families in this area and this will provide a safe option for children get outside and ride their bikes. keen to see some design detail for the treatment of the kenneth rd crossing. can we also get the street lighting upgraded to?</p>
132	<p>Excellent. We need more bicycle paths in the area to get to the shops safely.</p>

## Email Submissions:

## #133

An excellent proposal for Roseberry, especially as it

- is a busy street with many goods vehicles, not known for showing consideration to cyclists.
- connects to an existing shared path on Balgowlah.

However, the path continues to Kooralua, but stops short of connecting seamlessly to the another existing shared path along Condamine. Why has this opportunity been missed?

Further, while the work in underway (assuming it's approved) could you take the opportunity to reduce the street signage clutter in Roseberry? Some signs can be aggregated, but also (and despite Council's policy on the use of yellow lines) several *new* No Stopping poles have been erected - more than half a dozen in all? Can these be replaced with yellow lines? -

## #134

The above is NOT a good idea.

The volume of traffic & congestion in both directions of Roseberry Street is considerable, with vans moving in & out of retail parking bays e.g. Harvey Norman, & cars moving in & out of stores such as Bunnings & Woolworths: widening the shared path & thus reducing traffic flow even further would be catastrophic for drivers & pedestrians alike.

Pedestrians & cyclists need to be provided for in parks away from busy retail areas.

## #135

Attention Mr Phillip Gray – Transport Project Officer

Dear Sir

Thank you for your correspondence (Ref: TrP2021/144766) of 26 April in relation to the Path Upgrade – Roseberry Street, Balgowlah & Manly Vale.

I am writing specifically in relation to Strata Plan 32449, 34057, 34792 – 28-34 Roseberry Street, Balgowlah – 'Balgowlah Business Park' and attached is an image of the Council footpath which presents a very real safety issue and my question and request to you is for Council to address this trip factor at the earliest possible time.

I thank you for taking the time to consider that works could be undertaken in the near future and not have to wait for funds to be raised in this regard, as in the case of the proposed Shared Path.

## #136

Dear Phillip,

I don't have a problem with the Roseberry Street Shared Path as long as council doesn't take any more street parking away from Roseberry Street. The only reason this street has become so busy is because of Bunnings, Woolworths and then Aldi moving into the street. I am pretty sure Bunnings is going to take action soon with people parking in their car park for the cafes and not for shopping in the hardware store.

Kind regards,

## #137

SHARED PATH- Roseberry Street Balgowlah area.

There is approx. 90 plus submissions.

In looking at the 2 images provided its clear that the "SAFE and Separated" cycleway space for Adult Cyclists is NOT CATERED.

So the proposed 2.5 m wide "shared pathway" for Mums with Prams, and kids on bikes should be revised.

- The path could be widened to the KERB, but this requires "re-locating the trees and Undergrounding the Powerlines.
- To provide a "SAFE and SEPARAED" adults bike path, the adjacent KERB-SIDE parking lane could be re-purposed. It is safer than riding thru the normal street.
- So as the wider "options" have been omitted. So the 90 comments may be limited to the 2.5 m wide pathway proposal, instead of ALSO providing comment on a "SAFE and SEPARATED" bike path for adults.
- In Bourke Street Surry Hills is a Safe and Separated bike path that goes to the Supermarket near Taylor Square, and at Cleveland Street, Surry Hills. It is separated from the Footpath which is on both sides of the Street. Perhaps both over 2 m wide. The Trees in the Footpath width have largely been retained. The bike path is well used by cyclists, cargo bikes and various e-bike users, including e-trike users ( Mums with Kids in the Vehicle), and kids on journey to Bourke Street Primary School. The bike path is just one bike lane each way.
- So if Northern Beaches has a 30% reduction in GHG emissions Target, the more complex bike path deserves implementing.
- The below images illustrate Bike Paths in Copenhagen for adults and kids, unlike the Roseberry St proposed shared "Bike Path" within the Northern Beaches Area.



g)



### Super bikeways

Main article: [Copenhagen Super Bikeways](#)

*The Cycle Super Highways are a series of bike paths that connect Copenhagen to nearby suburbs.*

*This initiative began in 2009 when both Copenhagen and the neighboring cities realized that building new infrastructure would allow commuters to bike into the city and create more sustainable transportation options.*

*Since traffic planners believe it will be very difficult to raise the modal share substantially beyond the current 50% for its own residents, the city has begun targeting commuters entering the city from surrounding municipalities on trips in the 5–15-kilometre (3.1–9.3 mi) range, aiming to increase the number of bicycle commuters by 30% throughout the capital region.*

*The project involves a 500-kilometre (310 mi) network of 26 named and signposted routes following a uniform standard with high maintenance and good services such as automatic air-pumps and prioritized snow removal. While international media has created some hyperbole around the concept, labelling the network as bicycle superhighways, they are largely not grade separated. Rather the routes are regular cycle tracks receiving smaller targeted upgrades.*



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Document administration	
Version	1.0
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Status	Final
Related Projects	Northern Beaches Bike Plan

<b>ITEM 4.10</b>	<b>LARMER PLACE, NARRAWEENA - INSTALLATION OF NO STOPPING YELLOW LINE</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/152944</b>
<b>ATTACHMENTS</b>	<b>1 Plan</b> <b>2 Table of Consultation</b>

**GEOCODES: -33.745441, 151.278793**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding parked vehicles and trucks at the end of cul-de-sac on Larmer Place that obstruct access to and from driveways. Additionally, the presence of parked vehicles can result in difficulties for the completion of garbage collection services and creates a potential for the trucks to cause damage to parked vehicles.

### **LOCATION**

Larmer Place is a local road of 6m in width, carrying low volumes of two-way traffic. Larmer Place intersects Kerr Close on its eastern side and it ends in a cul-de-sac on its western side. There is no parking restriction on Larmer Place. There is no bus route that services Rickard Road.

### **ISSUES**

- Parked vehicles and trucks at the end of the cul-de-sac on Larmer Place obstruct access to and from driveways.
- The presence of parked vehicles can result in difficulties for the completion of garbage collection services and creates a potential for the trucks to cause damage to parked vehicles.

### **PROPOSAL**

Council has undertaken a review of the location and issues and proposes the installation of a 17m length of No Stopping yellow line from a point 5m east of the driveway to No.10 Larmer Place and the western side of the driveway to No.5 Larmer Place.

### **PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

Consultation letters have been distributed to 5 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

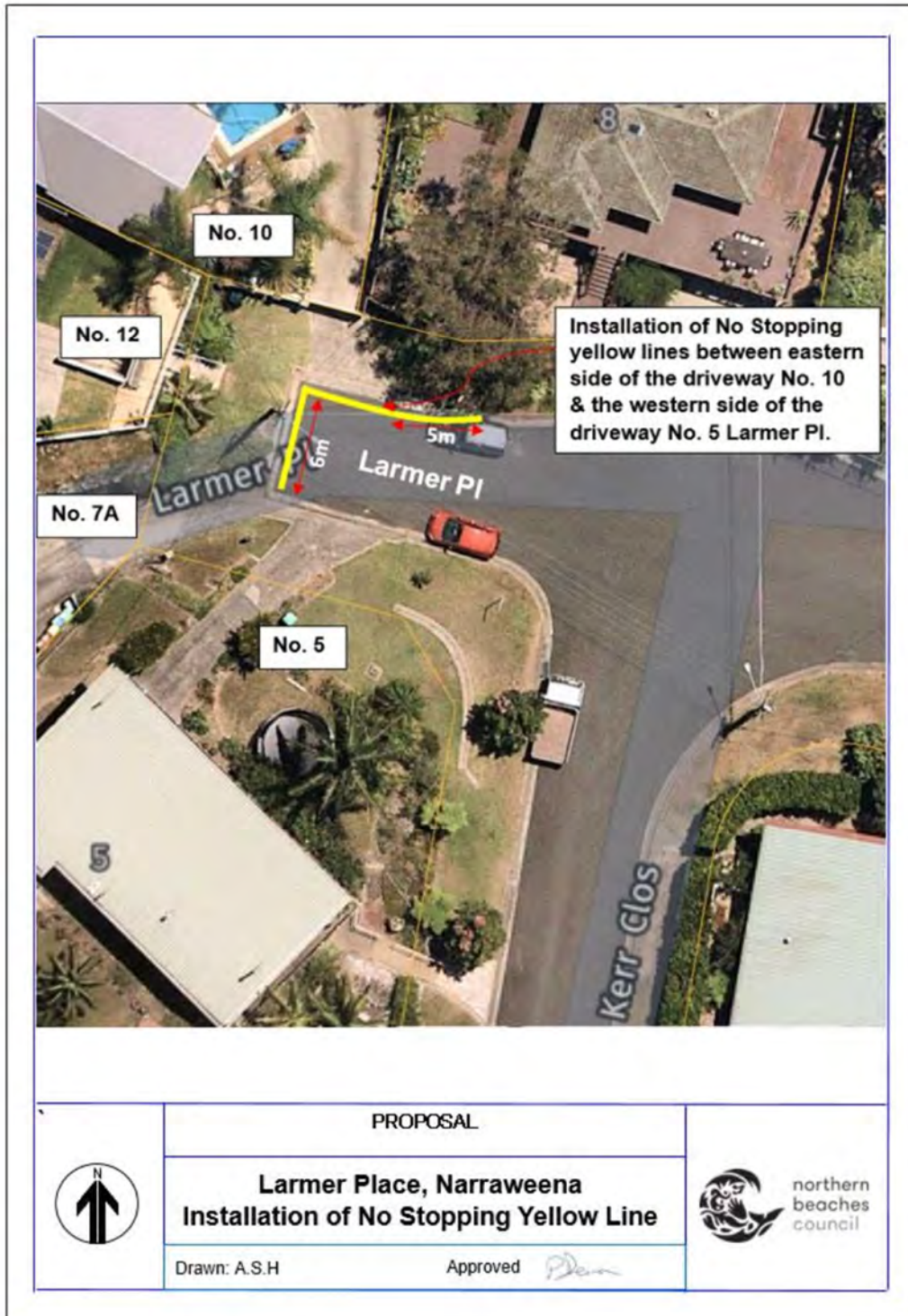
- A total of 3 submissions were received with 1 objection and 2 supporting the proposal.

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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports:

- A. The installation of a 17m length of No Stopping yellow line from a point 5m east of the driveway to No.10 Larmer Place and the western side of the driveway to No.5 Larmer Place.



<u><b>Table of Consultation</b></u>		
<b>Address</b>	<u>Larmer Place, NARRAWEENA</u>	
<b>Proposal</b>	<u>Installation of No Stopping Yellow Line</u>	
<b>Properties Consulted</b>	5	
<b>Responses Received</b>	3	
<b>Support</b>	2	
<b>Do Not Support</b>	1	
<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
Lack of Safety	Our neighbours are parking in the dead end, often trapping us in our driveway & causing issues when garbage trucks & delivery trucks come up our street.	Council proposes the installation of a 17m length of No Stopping yellow line from a point 5m east of the driveway to No.10 Larmer Place and the western side of the driveway to No.5 Larmer Place in order to resolve the mater and to improve the safety issues at the end of the cul-de-sac.
Lack of Safety	Unfortunately the steep gradient of the driveway has meant that the parking and garage is inaccessible for many vehicles, and it can be dangerous in wet weather. We are thus NOT usually able to park on our own property so we need to park and access our property from the street.	Long term parking of private vehicles must be on private properties as the residents are responsible to utilise garages and existing driveways for off-street parking in case of increased parking demands.

<b>ITEM 4.11</b>	<b>PITTWATER ROAD, CROSS STREET, ORCHARD ROAD AND MITCHELL ROAD, BROOKVALE - SHARED PATH</b>
<b>REPORTING OFFICER</b>	<b>ENGINEERING INTERN</b>
<b>TRIM FILE REF</b>	<b>2022/153523</b>
<b>ATTACHMENTS</b>	<b>1 Pittwater Road, Cross Street, Orchard Road and Mitchell Road - Plan</b> <b>2 Pittwater Road, Cross Street, Orchard Road and Mitchell Road - Engagement Report</b>

**GEOCODES: -33.765494, 151.274043**

## **REPORT**

### **BACKGROUND**

Council's 'Move – Northern Beaches Transport Strategy' in 2019 identified that shared paths are a way to reduce reliance on vehicles and encourage active travel and healthy recreation in walking and cycling. This project is part of Council's Tier 1 Regional Cycling route, which links Manly to Dee Why, through the Brookvale Strategic Centre. Additionally, Cross Street has become a popular route for bicycle riders to access Warringah Mall.

### **LOCATION**

- Pittwater Road is a state road with a speed limit of 60km/h through Brookvale. Additionally, there is a school zone with a speed limit of 40km/h between Pine Avenue and Chard Road.
- Timed Parking is available on both sides of Pittwater Road outside of the Clear Way times.
- Cross Street, Orchard Road and Mitchell Road are all local roads with speed limits of 50km/h and parallel parking on both sides of the road.
- This section of Pittwater Road, Cross Street, Orchard Road and Mitchell Road the industrial area of Brookvale with cafes, mechanics, gym facilities, hardware and shops.
- These streets have an existing 1.25m wide footpath on both sides of the roads.
- Mitchell Road has the 176 and 177 bus service.
- Pittwater Road has various bus services including the B-Line, 167, 172, 173, 178, 179, 180, 193 and the 199 bus routes.
- Major Trip attractors are located nearby to the proposed shared path. These include Warringah Mall, Brookvale Public School, Brookvale Oval, St Augustine's College, Freshwater Senior Campus, John Fisher Park and Netball Courts, Northern Beaches Secondary College and St Luke's Grammar School.
- There is an existing shared path on Harbord Road, Wattle Road and Pittwater Road, of which the proposal will connect to.

## ISSUES

- Closure of the signalised crossing at William Street has made it difficult for bicycle riders to access Warringah Mall.
- Concerns were raised about high volumes of traffic through Cross Street, Orchard Road and Mitchell Road.
- Concerns were raised regarding crossing opportunities at the intersections in the Brookvale Industrial Area.
- Concerns were raised regarding the safety on the footpaths by sharing it with bicycle riders.

## PROPOSAL

Council has undertaken a review of the location and issues and proposes to widen the existing footpaths on Pittwater Road, Cross Street, Orchard Road and Mitchell Road, to a 2.5m wide shared path. This will provide a safer footpath with additional space for all path users.

Additionally, refuge islands and kerb blisters are proposed for roads intersecting with Pittwater Road and Mitchell Road. This will improve crossing opportunities for cyclists and pedestrians. Kerb Blisters on Orchard Road, Sydenham Road, and Chard Road will remove approximately 6 parking spaces.

## PEDESTRIAN AND CYCLING IMPACT STATEMENT

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will improve cycling facilities and will connect to an existing shared paths on Harbord Road, Wattle Road and Pittwater Road.
- The proposal aims to improve pedestrian facilities and walking paths.

## CONSULTATION

Consultation was conducted through Council's Your Say page with 67 responses. Letters were distributed to 700 properties in the Brookvale Industrial Area. The full engagement report is seen as Attachment 2 – Engagement Report.

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## RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee supports:

- A. Construction of a 2.5m wide shared path on the northern side of Cross Street, from Green Street to Pittwater Road.
- B. Construction of a 2.5m wide shared path on the eastern side of Pittwater Road from Cross Street to Orchard Road.
- C. Construction of a 2.5m wide shared path on the southern side of Orchard Road from Pittwater Road to Mitchell Road.
- D. Construction of a 2.5m wide shared path on the eastern side of Mitchell Road from Wattle Road to Pittwater Road.
- E. Construction of a 2.5m wide shared path on the southern side of Pittwater Road from Mitchell Road to Harbord Road.
- F. Construction of a refuge island at the driveway to the carpark of Brookvale Bus Depot.

- G. Construction of a refuge island at the intersection of West Road and Pittwater Road.
- H. Construction of a refuge island at the intersection of Carter Road and Pittwater Road.
- I. Construction of Kerb Blisters on the eastern leg of the intersection of Mitchell Road and Sydenham Road.
- J. Construction of Kerb Blisters on the eastern leg of the intersection of Mitchell Road and Chard Road.
- K. Construction of Kerb Blisters on the eastern leg of the intersection of Mitchell Road and Orchard Road.



## CAUTION — SERVICES!!!

WARNING

- THE POSITION OF SERVICES SHOWN ON THE DRAWING ARE INDICATIVE ONLY AND HAVE BEEN PLOTTED FROM PLANS SUPPLIED ON PITS, TRENCHES, MARKER POSTS, SONS, ETC. HAVE BEEN PLOTTED ON THE DRAWINGS. WORKERS SHOULD AT THE TIME OF SURVEY BUT THE SURVEY DOES NOT INCLUDE DETAILED INVESTIGATION OR PHYSICAL VERIFICATION OF THE DEPTH OF THE SERVICES. INVESTIGATION OR PHYSICAL VERIFICATION OF THE DEPTH OF THE SERVICES SHOULD BE DONE PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ACTIVITY ON OR ADJACENT TO THE SITE TO OBTAIN UPDATED SERVICES DIAGRAMS THROUGH A DIAL-BEFORE-YOU-DIG SEARCH AND PHYSICAL SEARCH/VERIFICATION OF THE DEPTH OF THE SERVICES TO LOCATE AND DEPTH/VS OF ALL UNDERGROUND SERVICES, PRIOR TO COMMENCEMENT.

### LEGEND

NEW 2.5m THICK CONCRETE FOOTPATH

150mm THICK 25MPa COMPRESSIVE STRENGTH REINFORCED WITH SL/2 MESH PLACED 30mm BELOW TOP OF CONCRETE SLAB PLAIN CONCRETE BROOM FINISH.

180mm THICK 25MPa COMPRESSIVE STRENGTH REINFORCED WITH SL/2 MESH PLACED 30mm BELOW TOP OF CONCRETE SLAB. COVE FINISH.

CONCRETE BUSTER. SEE DETAILS.

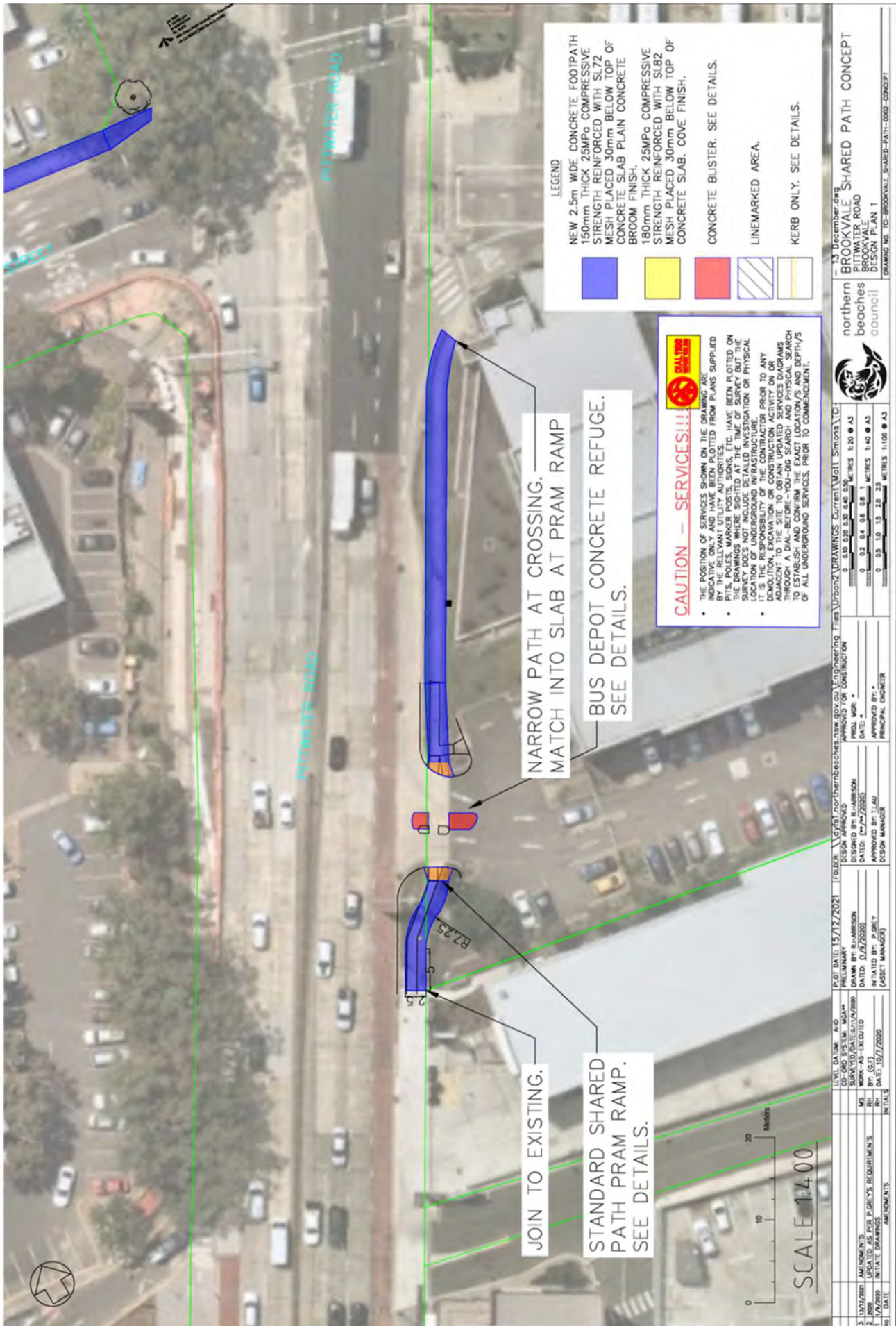
LINEMARKED AREA.

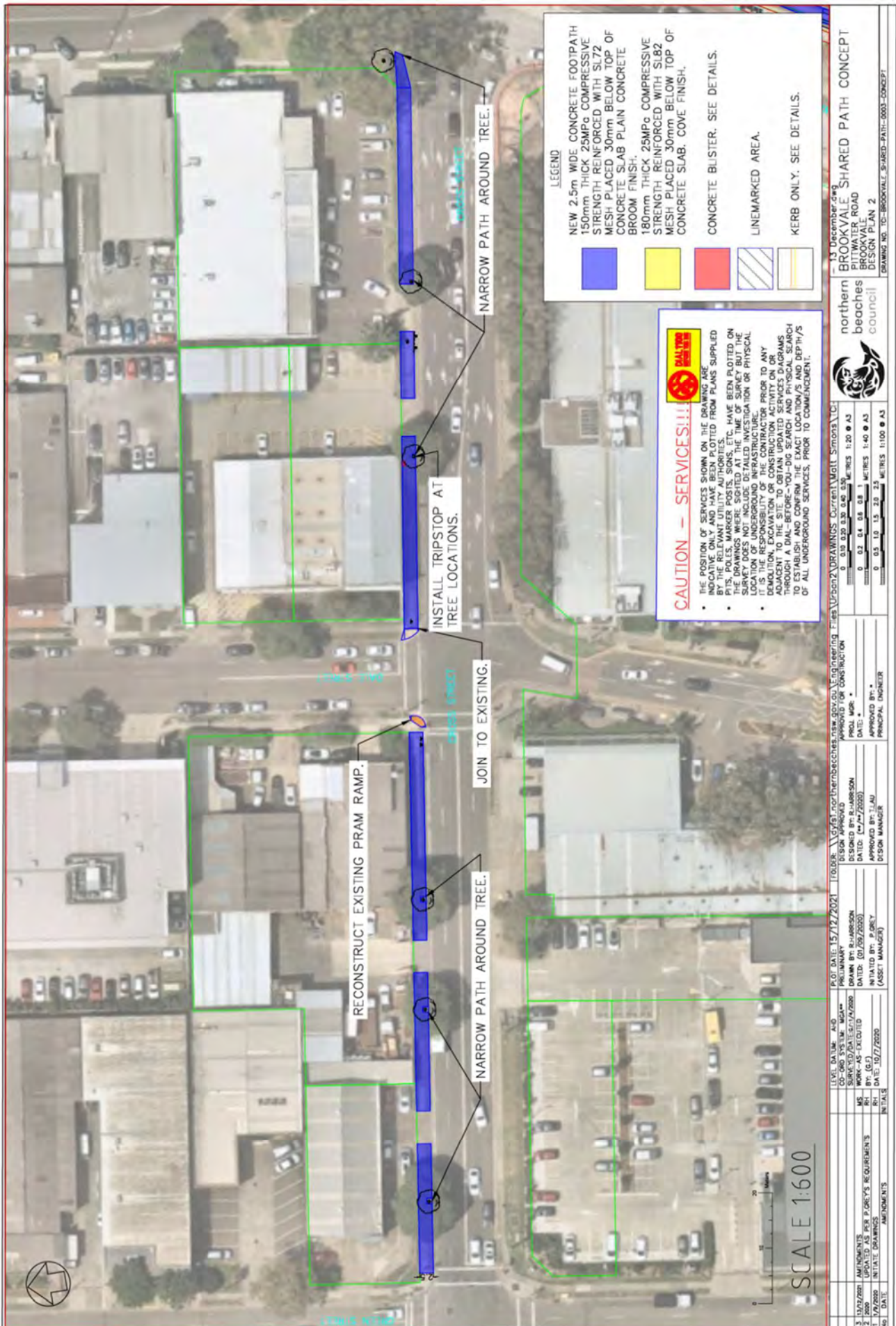
KERB ONLY. SEE DETAILS.

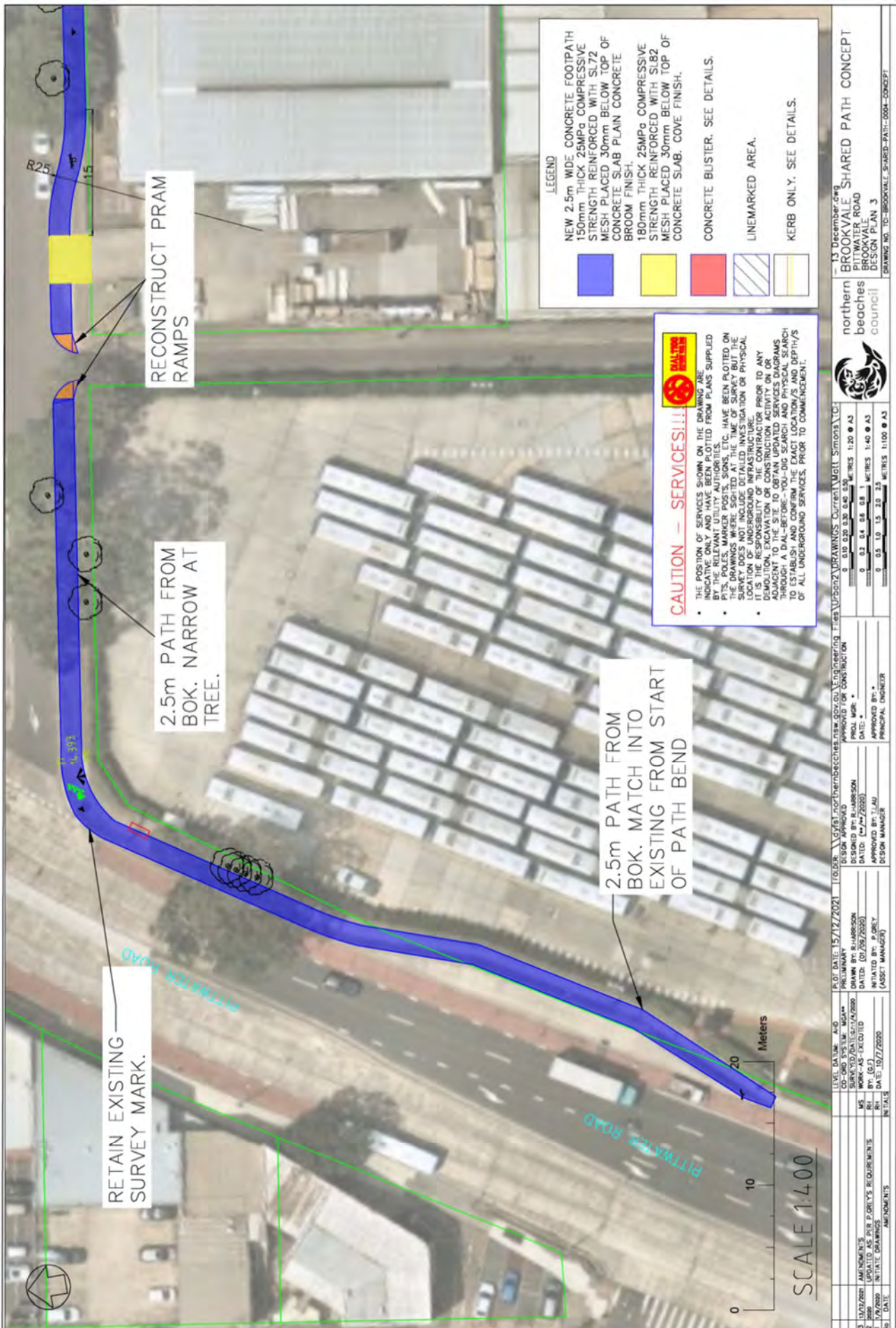
### DRAWING INDEX

DRAWING NO.	DESCRIPTION	ISSUE	REVISION
0001	COVER SHEET	APPENDIX'S	3
0002-0013	COVER SHEET	APPENDIX'S	3
0002-0013	COVER SHEET	APPENDIX'S	3
0001-0017	COVER SHEET	APPENDIX'S	3

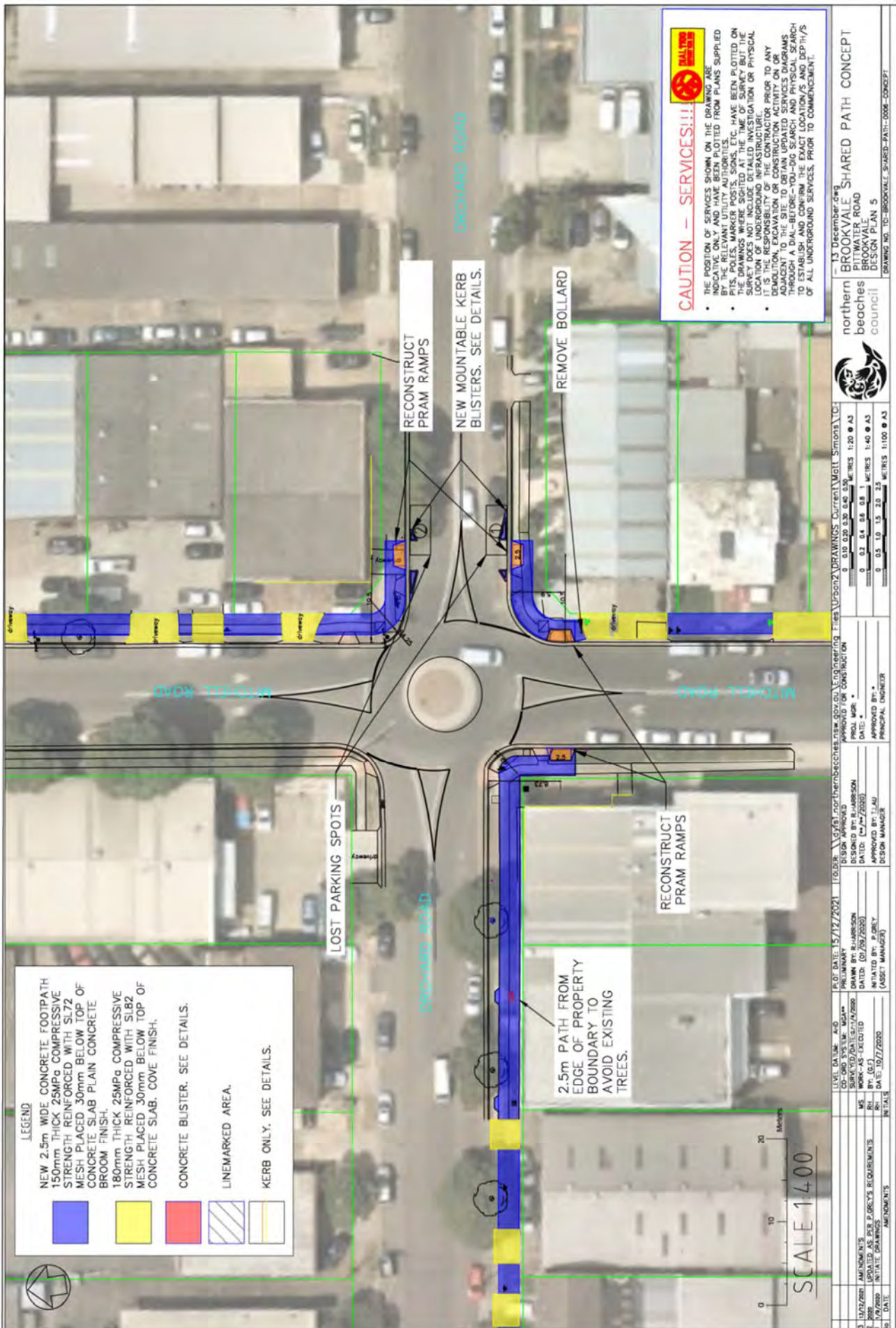
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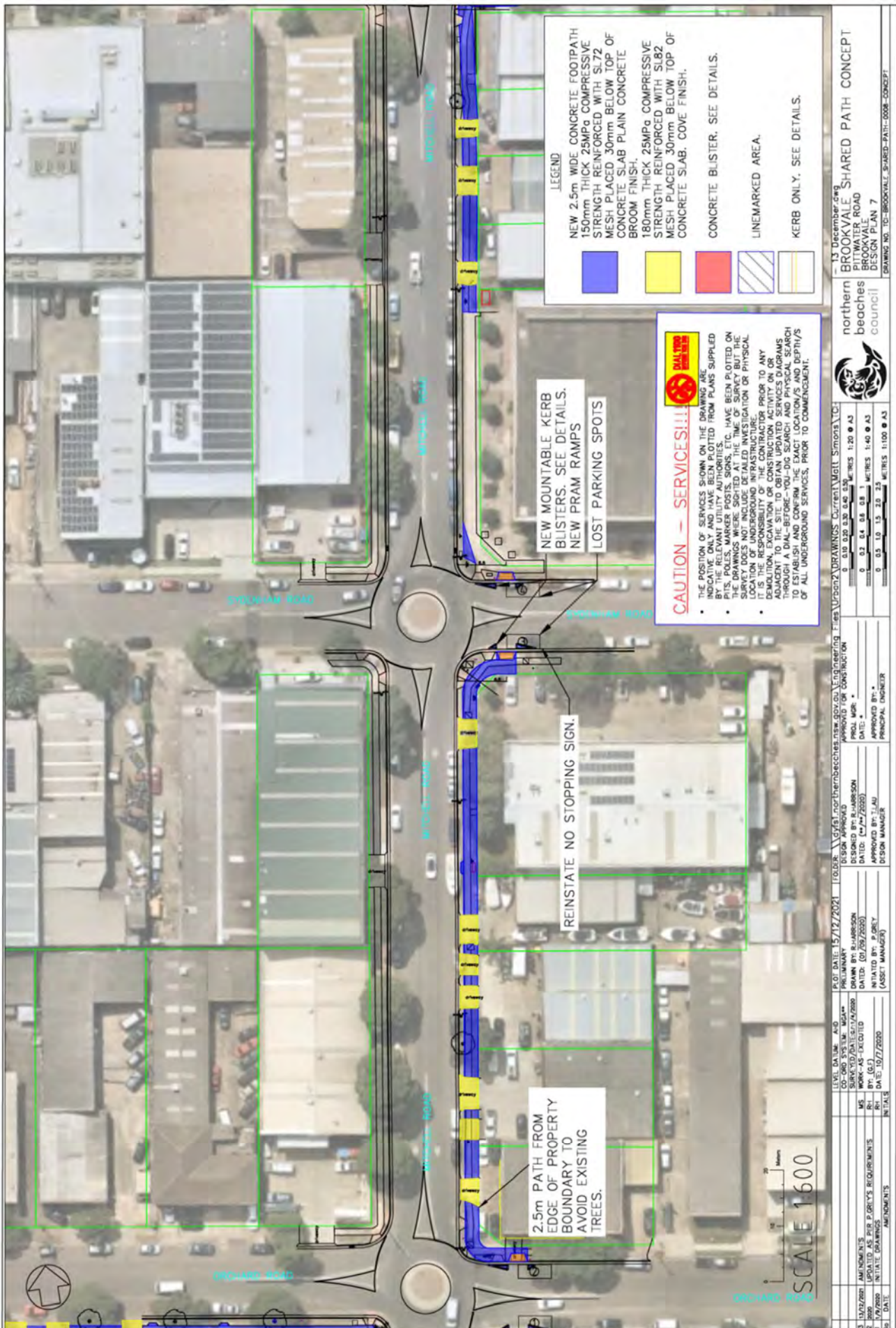


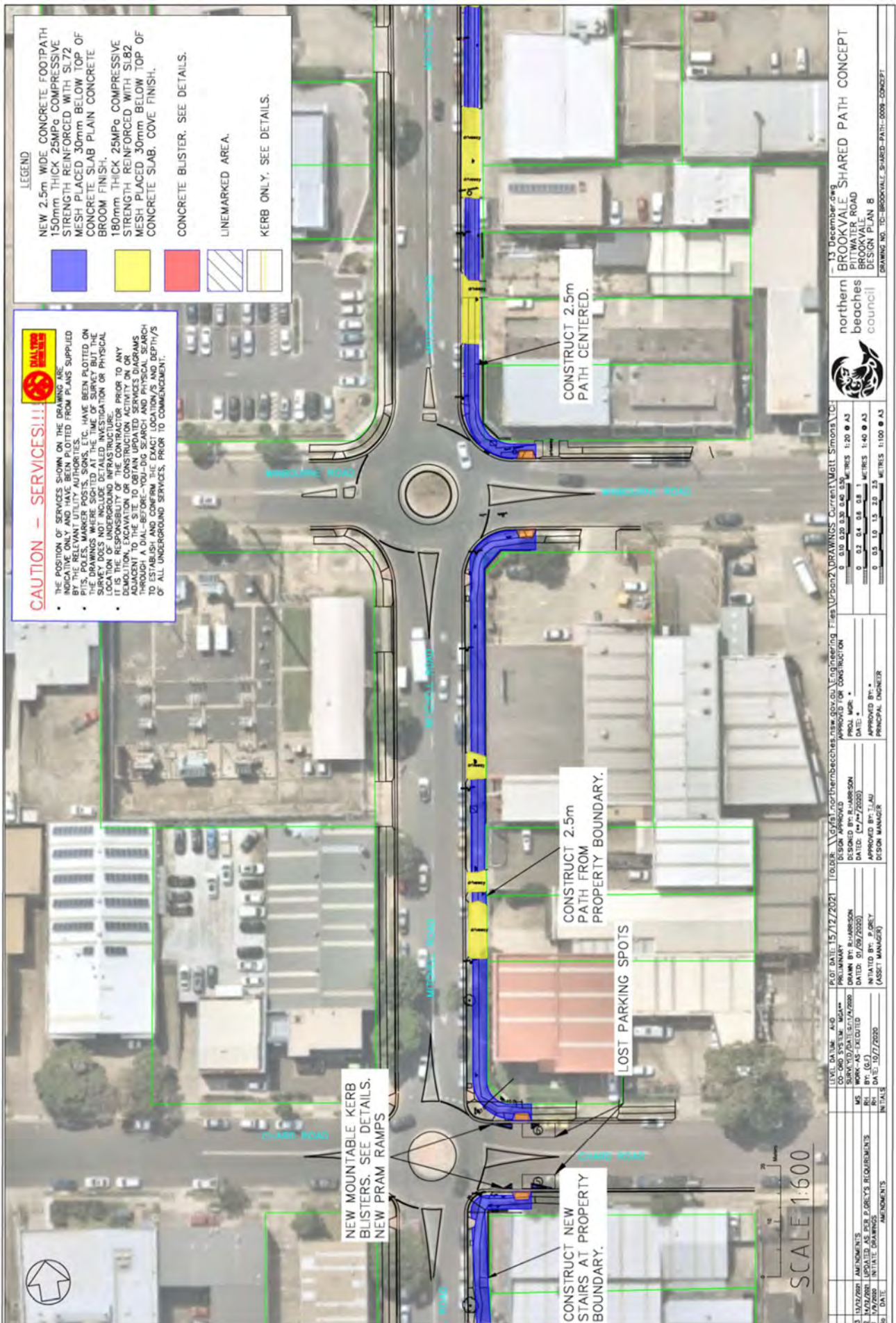




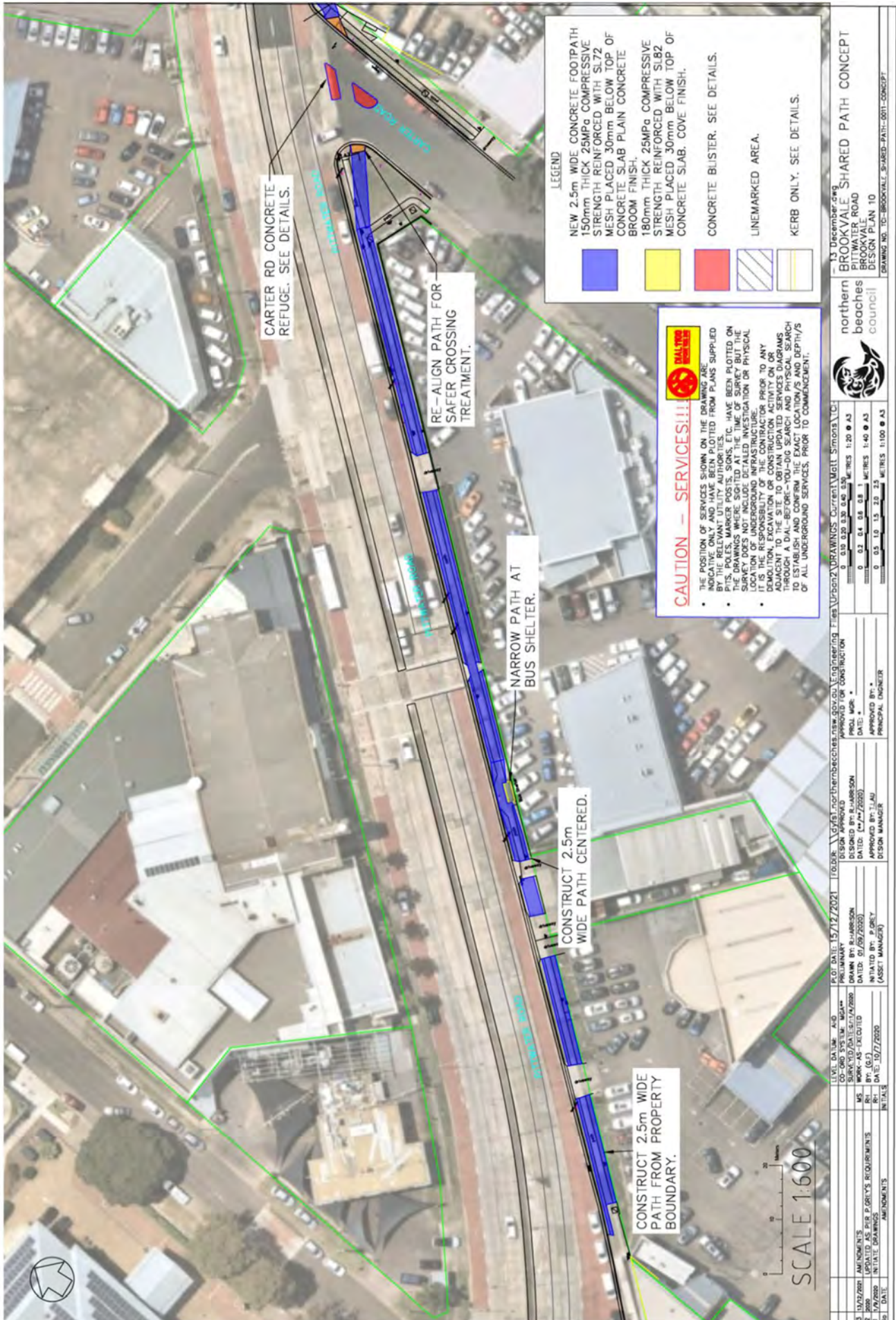


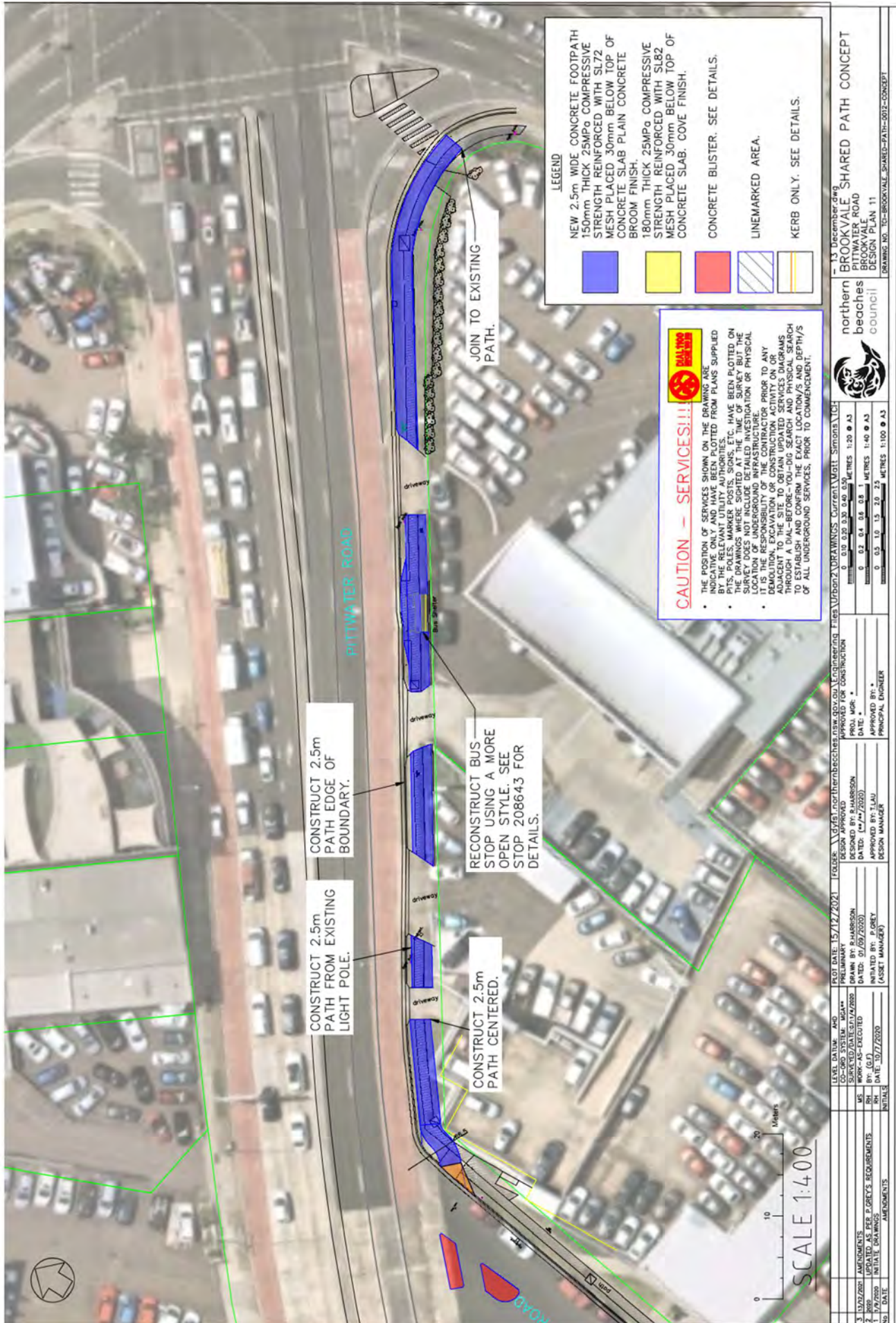


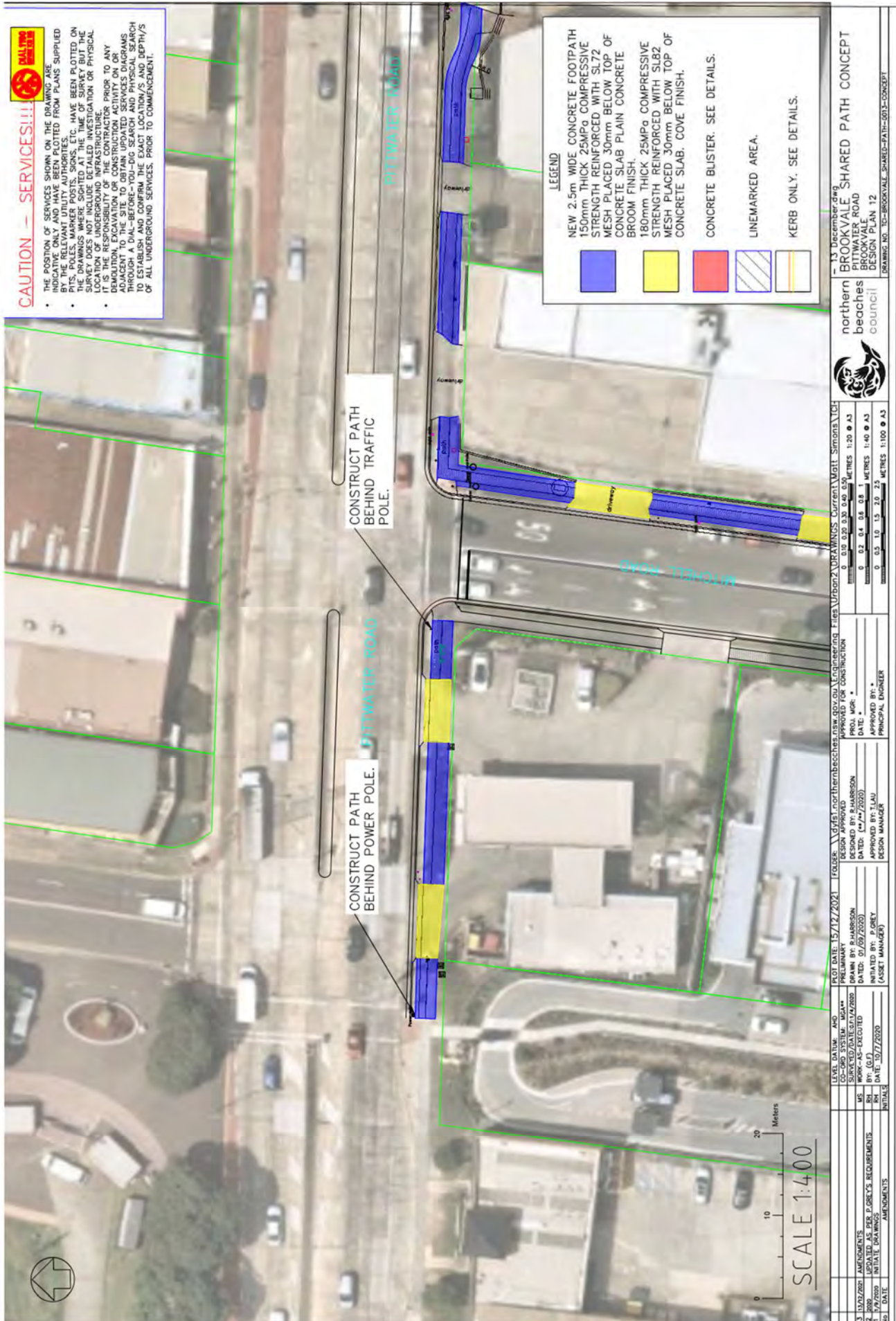


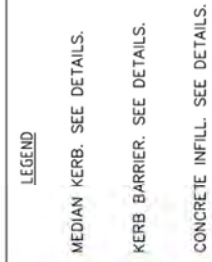
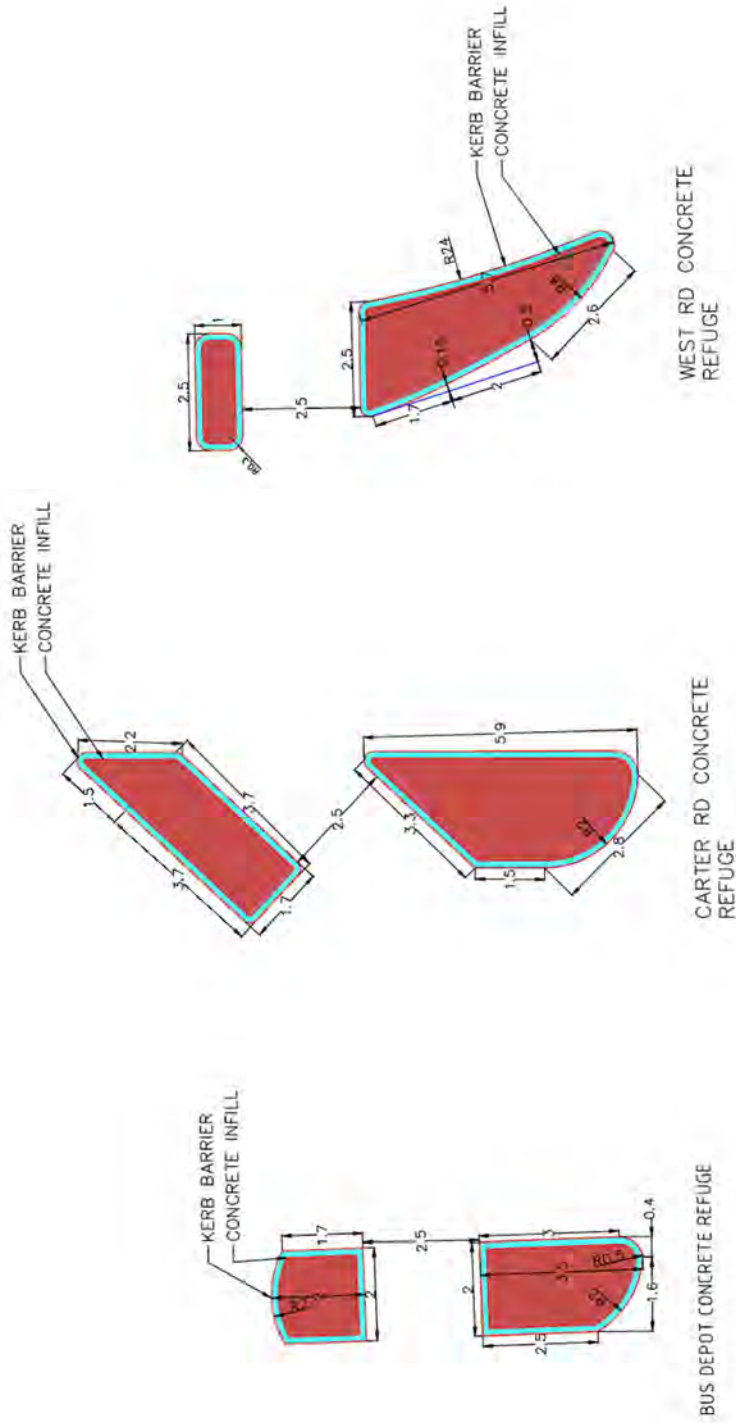





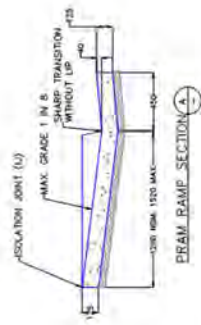
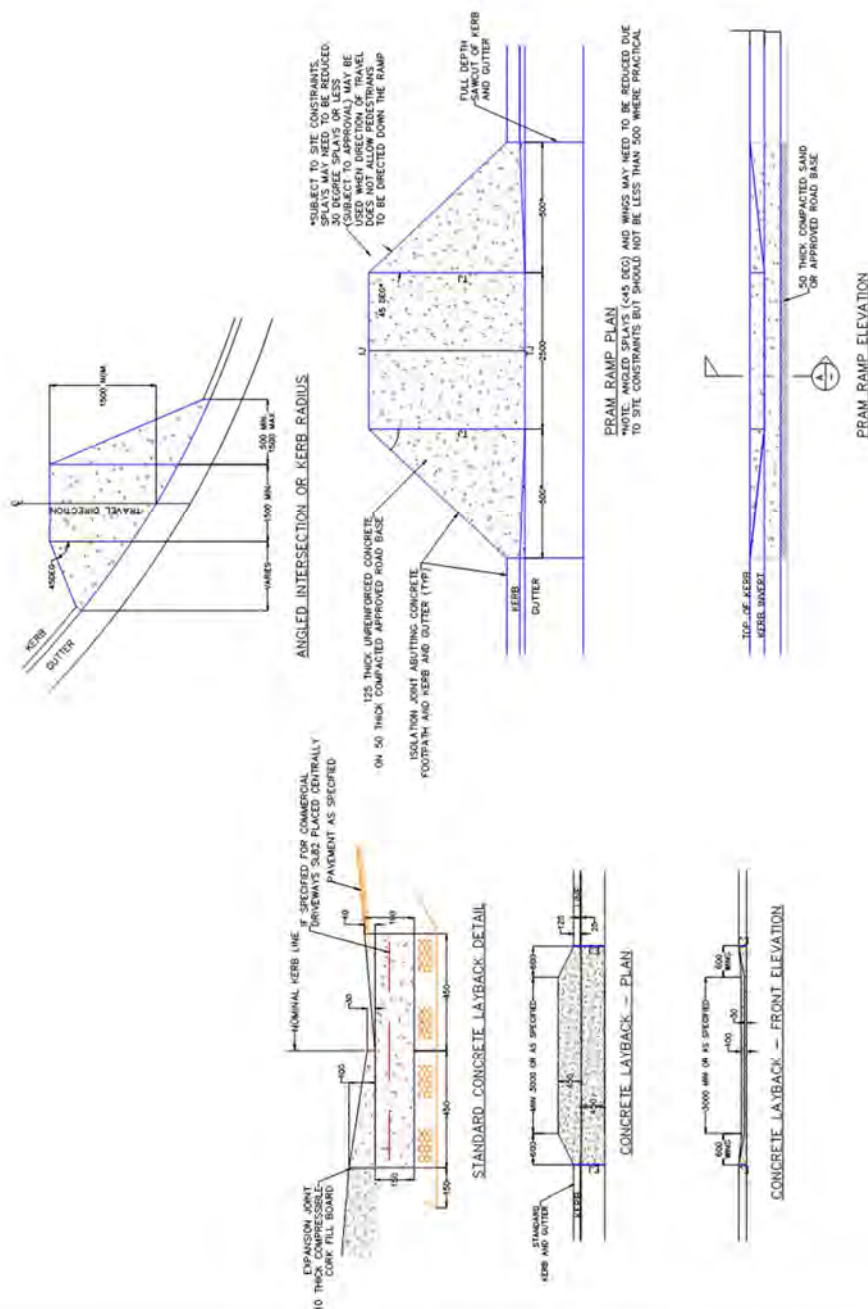








		- 13 December 2021 BROOKVALE SHARED PATH CONCEPT PITTWATER ROAD BROOKVALE REFUGE DETAILS DRAWING NO. TD-BROOKVALE-SHARED-PATH-2021-CONCEPT	
PLANNING DESIGNED BY: J. HARRISON DATED: 12/12/2020 APPROVED BY: J. HARRISON DESIGN MANAGER	PRELIMINARY PLANNING DESIGNED BY: J. HARRISON DATED: 12/12/2020 APPROVED BY: J. HARRISON DESIGN MANAGER	PLANNING DESIGNED BY: J. HARRISON DATED: 12/12/2020 APPROVED BY: J. HARRISON DESIGN MANAGER	PLANNING DESIGNED BY: J. HARRISON DATED: 12/12/2020 APPROVED BY: J. HARRISON DESIGN MANAGER
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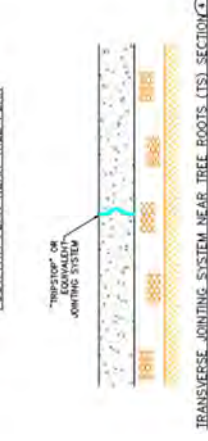
**PEDESTRIAN (PRAM) RAMP NOTES**

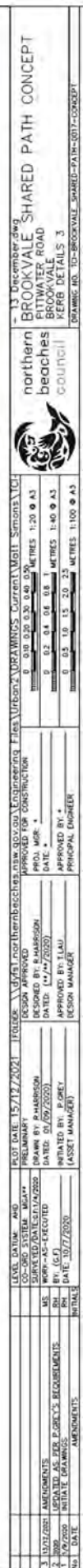
1. PEDESTRIAN RAMPS SHALL BE 125mm THICK, UNREINFORCED AND CONVE SURFACE FINISHED ON 50mm THICK SAND BEDDING OR APPROVED ROAD BASE.
2. PEDESTRIAN RAMPS SHALL BE FINISHED WITH A FINISH THAT IS SUITABLE FOR THE INTENDED USE OF THE RAMP.
3. ALL EXPOSED EDGES SHALL BE NEATLY FINISHED WITH AN EDGING TOOL FORMING A CHAMFER.
4. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 25MPa AT 28 DAYS AND MAXIMUM SLUMP OF 80mm.
5. THE CONTRACTOR SHALL PLACE A 50mm DEEP SAW CUT IN THE OUTER INVERT TO REMOVE THE INTEGRAL KERB AND GUTTER.
6. THE GUTTER SHALL BE STEEL FLOAT FINISHED. ALSO REFER TO JOINT DETAILS.
7. COMPACTION EQUIPMENT UNTIL IT SHOWS NO SIGNS OF MOVEMENT OR AS DIRECTED BY COUNCIL.
8. CONSIDERATION OF THE GENERAL MOVEMENT OF PEDESTRIANS, THE LOCATION OF MARKED PEDESTRIAN CROSSINGS AND THE POSITION OF ANY EXISTING OBSTACLES MUST BE TAKEN INTO ACCOUNT WHEN DESIGNING AND CONSTRUCTING PEDESTRIAN RAMPS. PEDESTRIAN RAMPS MUST ENSURE THAT USERS ARE NOT PUT AT RISK FROM TRAFFIC OF ANY KIND BEARING IN MIND THAT A DISABLED PERSON'S CROSSING OF A ROAD MUST BE DONE WITHOUT THE ASSISTANCE OF ANY OTHER PERSON. HOWEVER PEDESTRIAN TRAVEL WHERE-EVER PRACTICABLE.
9. PEDESTRIAN TRAVEL WHERE-EVER PRACTICABLE.
10. COUNCIL INDICATORS SHALL BE INSTALLED AT SIGNALISED INTERSECTIONS, T-JUNCTIONS AND HIGH PEDESTRIAN USE AREAS ONLY OF WHERE INSTRUCTED BY COUNCIL AND IS TO BE INSTALLED IN ACCORDANCE WITH AS1624.
11. THE DESIGN OF THE RAMP SHALL BE IN ACCORDANCE WITH THE DESIGN OF THE RAMP REFER TO AUSTRALIAN STANDARD AS1428.1-2009 "DESIGN FOR ACCESS AND MOBILITY" FOR FURTHER DETAILS.
12. ALL DIMENSIONS SHOWN ARE IN MILLIMETRES UNLESS NOTED OTHERWISE.

LEVEL DATUM AND CO-ORD. SYSTEM		DATE	15/12/2021	PROJECT	15/12/2021	DESIGN APPROVED	DESIGNED BY	DATE	15/12/2021	INITIATED BY	DATE	15/12/2021	INITIALS	DATE	15/12/2021	INITIALS	DATE	15/12/2021	INITIALS
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**northern  
beaches  
council**

**BROOKVALE SHARED PATH CONCEPT**  
**PITTWATER ROAD**  
**BROOKVALE**  
**KERB DETAILS 1**  
DRAWING NO. 10-BROOKVALE SHARED-PATH-001-CONCEPT

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## Community and Stakeholder Engagement Report

### Brookvale Shared Path

Impact level: Four

Report date: 15 October 2020

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#### Contents

1.	Summary.....	2
1.1.	Engagement date.....	2
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1.3.	How we engaged .....	3
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4.	Findings .....	5

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## 1. Summary<sup>1</sup>

This report outlines the community and stakeholder engagement conducted as part of the Brookvale shared path project.

The community consultation revealed that most respondents were supportive of the concept of a shared path through Brookvale, linking Warringah Mall and the B-Line to Dee Why, describing benefits to the community including a safer walking/cycling option and providing 'family-friendly' connections to shops, transport and schools.

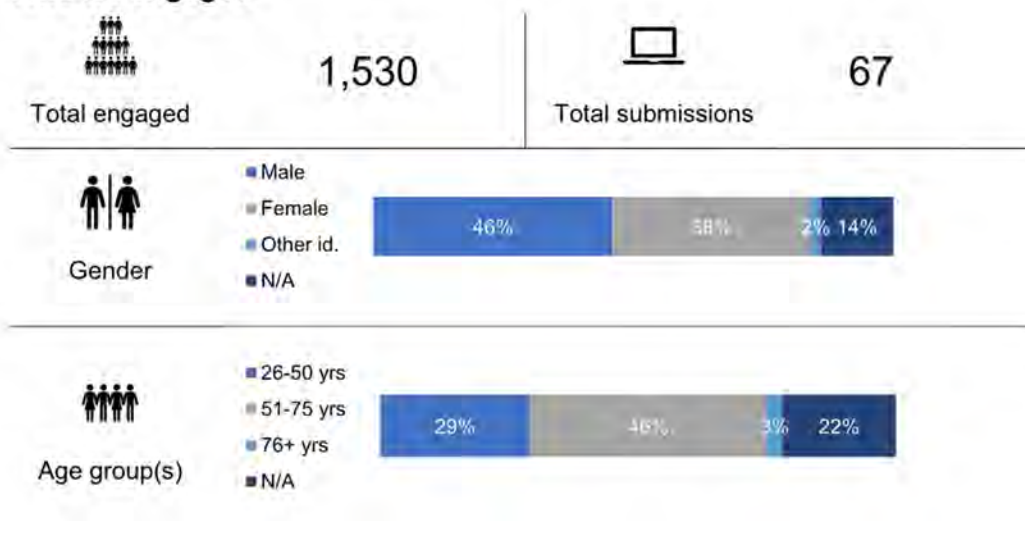
The predominant theme that emerged from the feedback was the issue of safety. Respondents felt that the proposed route through a highly industrialised area, with constant traffic entering and exiting businesses posed a risk for cyclists, as did the numerous road crossings and roundabouts along the route.

Other themes included access issues to Warringah Mall and the potential for conflict between users of shared paths. Some respondents would prefer to see safer, purpose-built separated cycleways rather than footpath widening as the solution.

### 1.1. Engagement date

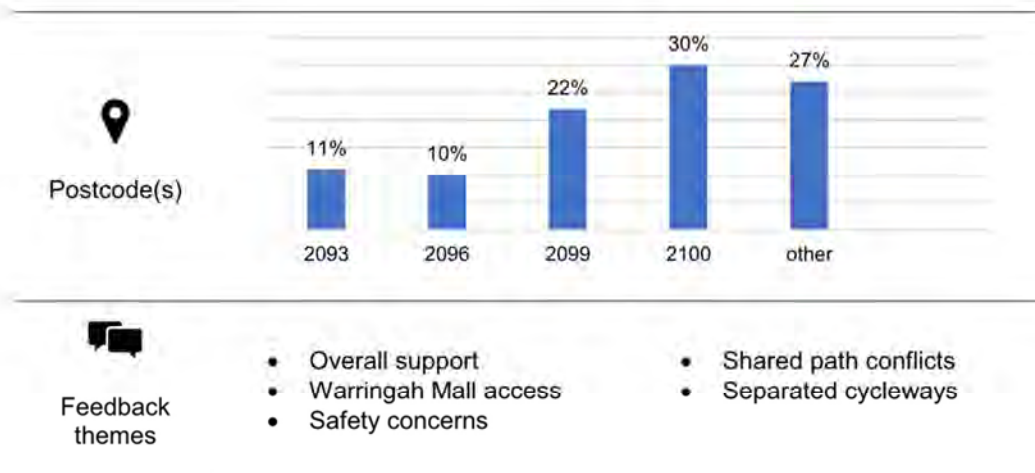
21 August to 25 September 2020

### 1.2. Who we engaged<sup>2</sup>



<sup>1</sup> Community and stakeholder views contained in this report do not necessarily reflect the views of the Northern Beaches Council or indicate a commitment to a particular course of action.

<sup>2</sup> No demographic data was captured for respondents who contributed feedback through direct emails or letters.



### 1.3. How we engaged

	Visitors: 1,530	Visits: 1,789	Av. time onsite: 59 seconds
Your Say			
	Mail out: Brookvale businesses and residents along proposed route		Distribution: 700
Print media and collateral			
	Community Engagement newsletter: 2	Distribution: 20,000	
Electronic Direct Mail (EDM)	Council eNews: 1	Distribution: 70,000	
	Stakeholder EDM:1	Distribution: 507	

## 2. Background

This project is part of the implementation of the Bike Plan to provide a safe and connected active transport network on the Northern Beaches.

The proposal is to build an extension of the shared path network to connect Brookvale B-Line and Warringah Mall to Dee Why. The 2.5km shared path would run along Pittwater Road, Orchard Street, Mitchell Road and Cross Street.

Shared paths provide wider and safer walking and cycling routes allowing greater accessibility for prams, wheelchairs and mobility scooters.

This project is subject to funding prior to construction.

## 3. Engagement approach

Community and stakeholder consultation for the Brookvale shared path project was conducted over five weeks, from 21 August to 25 September 2020, and was planned, implemented and reported in accordance with Council's [Community Engagement Matrix](#) (2017).

Project information and an opportunity to engage was included on the Council's Have Your Say project page. Behavioural economics principles guided the development of the online content to ensure clear and consistent information was provided to our community. A concept of the proposed route was used to support the page content.

Feedback was primarily collected through an online comment form with email and written responses also accepted.

To promote the engagement to local residents and businesses, a notification letter was posted to 700 people along the proposed shared path route in Brookvale.

The public exhibition was also promoted by an email to the 507 subscribers of the Bike Plan.

The project was included in the Community Engagement and Council's enewsletters to reach the broader community.

### 3.1. Engagement objectives

- Build community and stakeholder awareness of participation activities (inform)
- Provide accessible information so community and stakeholders can participate in a meaningful way (inform)
- Identify community and stakeholder concerns, local knowledge and values (consult)

## 4. Findings

Overall, the majority of the feedback received was supportive of the Brookvale shared path concept, with some comments suggesting slight changes in the route, additional connections or extensions to the shared path network.

The supportive comments noted that Brookvale was becoming busier and more vibrant with more residents and an increase in food outlets and cafes in the area and that building a shared path would encourage particularly families with children, to ride or walk to shops, cafes, schools and daycare centres.

Safety was a re-occurring theme in the feedback. Many comments said that the proposed route through a busy commercial area (and in particular Mitchell Road) posed risks to cyclists due to the high volume of traffic and with cars and trucks constantly driving in and out of driveways. Road crossings, roundabouts and intersections were also considered dangerous for cyclists.

Access issues at Warringah Mall was mentioned several times in the comments. The pedestrian crossing at William Street has been removed and respondents believe taking the lift and footbridge over Pittwater Road is not a convenient option for bike riders. A couple of comments thought that having to ride to Cross Street to access the crossing at the traffic lights could be hazardous.

There were general comments regarding the potential for conflict between users of shared paths. Some felt that paths should be for pedestrians only, there needs to be clear etiquette rules for users and cyclists should use a bell.

Some respondents would prefer to see safer, purpose-built separated cycleways rather than footpath widening as the solution.

Table 1: Summary of the main themes, comments and Council's response

Theme	What we heard	Response
Overall support	Community benefits in encouraging active travel.	Noted
Alternate route suggestions	Additions, extensions, network connections	Other routes and locations have been identified as future safe cycling network connections in the Northern Beaches Bike Plan. These will be addressed in future projects. This project specifically addresses the need to access Brookvale B-Line from the north, access the at grade crossing of Pittwater Road at Cross Street and provide a safe cycling route that provides access to areas of Brookvale.


Access to Warringah Mall	The closure of the signalised pedestrian crossing at William St has made it very difficult to access Warringah Mall by bicycle.	This issue is a main driver of this project. A safe cycling connection to the Cross Street intersection of Pittwater Road is a main objective of this project. This will allow an alternative for people riding bicycles to cross Pittwater Road without having to use the lift.
High levels of vehicle traffic	Some respondents considered the area to be too dangerous for bicycles as it is a very busy commercial area with a lot of traffic and large vehicle movements.  Mitchell Street was mentioned as a particularly dangerous street for a shared path as it's very busy	Traffic congestion is a reason to provide an alternative transport option and in particular, a safe cycling option that is separated from the road.
Safety	Some comments expressed concerns that the road crossings, intersections and roundabouts are too dangerous for cyclists.	The proposal will use existing crossing points. These will be made safer by applying treatments such as kerb extensions or additional line marking.
Designated, separated cycleways	A couple of respondents thought the proposal should consist of a higher quality separated cycleway rather than a shared path. This would provide a better, safer facility for cycling.	It is agreed that a separated cycleway would be the most desirable treatment for a safe cycling route, however this treatment would require the reallocation of road space or on-street parking. Both of these options are not seen to be viable at this point in time. A shared path will provide a safe cycling option that is not currently available.
Shared path conflict	There were a few concerns in the comments that shared paths are dangerous due to the conflict between pedestrians and bicycle riders.	This is a concern that is often raised regarding shared paths.  Transport for NSW Centre for Road Safety released a discussion paper on shared paths in 2015. The concluding remarks, based on evidence, stated that shared paths represent a relatively low safety risk.

## Appendix A - Online submissions received (verbatim)

No.	Comments
1	A good first step but there is an urgent need of an extension to connect to Allambie Road in some way so bike riders can cycle from the beaches areas to the NB Hospital via Brookvale. Riders are now disadvantaged by the closure of the Condamine St crossing. Consider upgrading the footpath along Pittwater Rd. (W) from Condamine St to Old Pittwater Rd/Smith St. and connecting it to your proposal.
2	Actual entry/exit from Warringah Mall car parks on bicycle is quite tricky. At the moment, to exit the mall I ride on the road to Sydenham Road to then walk across the pedestrian crossing to get back to Orchard St. This isn't an ideal route due to the busy car traffic on these roads. Could a bicycle crossing be made from the proposed Pittwater Road cycle track to get inside the car parking region of Warringah Mall? Or perhaps at Cross St? This would be very helpful.
3	Any increase in designated bike paths is a great initiative. Should this be aimed at helping communities connect with public transport, bike racks will also be required.
4	As a Brookvale resident I am very supportive of any upgrades to the footpaths in our area, and better access to shared paths. I have a 7 year old and a 4 year old daughter and we would love to be able to ride our bikes to Curly for beach days, Nippers, parkrun, the new skate park and Auskick. My eldest and I have managed it a couple of times but this has required a lot of riding on footpaths because she is far too young to ride the busy roads of Brookvale even with an adult with her. Our area is becoming increasingly busy with traffic due to the mall, B-Line commuter carpark, increasing numbers of cafes and breweries & the COVID testing clinic. We also have a increasing number of young families living in Brookvale, making their way along the footpaths along Old Pittwater Rd and other roads to get to Brookvale Public School. It would be wonderful if the key routes to BPS could be added to the Shared Path proposal, as it would be wonderful if the kids could ride to school safely (and would reduce the current traffic jams being experienced along Old Pittwater Rd and Beacon Hill Rd every morning and afternoon). It would be great to see Council provide more options for Brookvale residents wanting to get out and about but not in a car.
5	As a brookvale resident I think this is a wonderful idea. I feel hesitant to travel via bicycle at the moment, and feel that the riskiest part of my journey is travelling from my home to the closest connecting shared bike path. At the moment I feel people unofficially use the path anyway as it is a significantly safer option than navigating the main road or industrial back roads to travel between warringah mall to dee why.
6	As a regular bike rider (daily!), I welcome any extension and improvements made between Dee Why & Brookvale bike paths. I often commute from Cromer to Manly (& then catch ferry to Circular Quay). The current bike paths between Dee Why beach & Manly Wharf are good... however they involve travelling some quite steep hills. Bike routes through the Brookvale industrial area would be ideal as much flatter & more accessible for pedestrians and bike riders alike.
7	Can we leave the path in shackel Av alone as parking is bad enough in the st it will only make it worse if we widen it
8	Could be Dangerous through a very busy Brookvale industrial area with trucks and cars in and out of driveways . Would be good on a weekend .
9	Could do with more information. What are the future shared path markings for? What is the purpose of the shared path, ie is it for locals or people getting off buses or commuting, it's not clear to me.
10	For shared paths to be used by cyclists there needs to be traffic lights with bike crossing priority for at intersections. Most adult riders will still use the road if they have to dismount at every intersection.
11	Getting to and from the Mall from William St with a bicycle is a nightmare now you can't cross at the crosswalk. There are also no signs on Condamine indicating how to get on the bike path along Burnt Bridge. So cyclists that don't know will ride along the road. Three sets of lights don't work at The Strand over the southern most crosswalk.
12	Great idea

No.	Comments
13	Great idea - what with all the leisure & hospitality businesses having opened up & more into the future, be great to make Brookvale the link between Manly & Dee Why & the surrounding suburbs more family friendly & looking to the future of the area.
14	Great idea- safer for families, delivery riders and drivers...
15	Great idea - While its much nicer biking along the coast, it can be faster along the main road.
16	Great idea, but only when you make sure the other side of the footpath cannot be accessed with any bicycles, put a sign up please. Especially the delivery drivers are the worst. To be fair to bicyclist make sure paths or shared paths do not end in no-where and are connected together.
17	Great stuff. I suggest to include a shared path in front of the Brookvale Bline busstop to allow cyclists from Manly to reach Cross St. (Linking the current yellow and red line). And yes this means a joint responsibility of cyclists and bus travellers to manage the space in front of the bus stop. Not rocket science.
18	Great to see Council implementing more walking and cycling links in the LGA. I currently use Harbord Road and back streets through Brookvale to get to Warringah Mall, but the Harbord Road shared path has a lot of obstacles in it - I don't ride it downhill as it is more dangerous than riding on the road. It would be even better to see Council considering reallocating road space to bicycle riders, rather than forcing pedestrians and bicycle riders to share limited space while maintaining multiple travel lanes for motorists.
19	Great! The more off-road bike paths, the better! Thank you :)
20	Great! We enjoy cycling on the Northern Beaches and safe off road paths help a lot.
21	Has to be done asap
22	Hi, I live in this area with kids that go to schools and daycare locally. I fully support Councils push for 2.5m shared paths. This north / south link looks a good one. I think Council should look to prioritise an east / west link like you have proposed a future path on Winbourne Rd which would be great. This would provide benefit to multiple schools, the stadium and green space in the east. Keep up the good work and keep me posted. Cheers,
23	Hi. This is as a great initiative. Fills in done missing links to get kids and adult safely to schools and sporting facilities. Also helpful as an increasing number of afterschool activities are located in the industrial estate and a long walk from bus stops.
24	I am a Brookvale resident and cyclist. Overall I think this is a good idea but safety needs to be a priority, both for pedestrians on shared paths and for cyclists regarding motor traffic. There is little pedestrian traffic on the paths proposed. The highest risk is going to be Mitchell Road and its junctions, especially at roundabout junctions given the high volume of tradie traffic in the area. What is the plan to safeguard cyclists at these intersections?
25	I cycle this route once or twice per week. I'm in favour of the cycle-way as proposed. I would definitely use it when cycling North. However, I would probably continue to use the road when travelling South, for speed.
26	I love the idea of increasing the walk ways especially along mitchel rd. It would also be great if mitchel road was made safer. It's extremely challenging to Cross all the side streets as it's a very busy strip of road (especially during peak times, with afternoons the worst)
27	I ride daily to Warringah Mall from North Manly along Pittwater Rd since they have closed the crossing at Warringah Mall it's very unfriendly for bike riders to go to the Mall there are many bike stands in the Mall but no right way to get into the mall by bike the lift is not friendly for bike riders as the bike takes up to much room & you have to be in the lift twice. You ride up to Cross St. the path is narrow & all your doing to crossing at lights which you only had 1 crossing to deal with. Please help the bike rides that could from Manly to Warringah Mall with a bike path.
28	I suggest we reconsider the need for a route along Mitchell Rd. which is one of the busiest and most dangerous roads in Brookvale. The concept is for a shared path but, in reality, almost no one walks along Mitchell Rd. so the path will be almost exclusively used by bicycle riders. However, serious bicycle riders (those mainly riding to work rather than for recreation) will always choose the fastest route and shared paths, although safer, are slower. A shared path along Mitchell Rd. has to cross several roads and the intersections are mainly roundabouts so I'm not sure exactly where the shared path will cross. As a regular bike rider in that area I would steer clear of Mitchell Rd. It is quicker to ride along Pittwater Rd than Mitchell Rd. (at most times of the

No.	Comments
	day). If a route has to be found between Cross St and places to the east, why not just extend the path along Orchard St. along the route shown with a red dotted line as a future extension of the path, and connect to Harbord Rd. which is a much safer route. I see no benefit in using Mitchell Road.
29	I think it's a fantastic idea. Brookvale has such an increasing amount of people sharing the pathways and this can only be a positive for the community.
30	If the "shared" path is anything like the "shared" path around Narrabeen Lagoon, then pedestrians will be taking their lives in their hands. Kamikaze bike riders travel at excessive speeds and obviously cannot read the signs about using bells or other warnings for pedestrians. Consideration MUST be given to active policing of the bike riders on shared paths to ensure they act responsibly and not as if the path belongs to them alone. These observations are not based on hearsay, they are based on experience - my partner walks our two dogs (on leash) twice a day and at least weekly tells me of near misses and/or verbal abuse from bike riders. And lest I am accused of being a bike hater, I have two of my own.
31	I'm looking forward to this. Riding through Brookvale is always a challenge. Pittwater road is very dangerous and Mitchell road very busy at peak hour so a shared path is very welcome. I'm keen to know the proposed crossings of busy roads such as Winbourne road, having used the path along Harbord Rd, crossing Headland road is particularly dangerous.
32	Looks good.
33	Looks like a good plan, carry on.
34	Love more off road cycle paths Brookvale to curl curl and Brookvale to manly fairly flat areas
35	Mitchell Road can be excessively busy, especially in peak afternoon when most of the trades finish in the area. On one hand it will be safer for bicyclists to be on the shared path, on the other hand the roundabouts can be aggressively used by cars/other vehicles and perhaps it would be safer for bicyclists to use another route. At least on the major roads (Pittwater & Harbord) there are traffic lights and no roundabouts that get aggressively used.
36	Pedestrians and bikes don't work. The bike riders come through at full speed expecting everyone to jump out of their way. They often don't ring bells or don't even have one on their bike. The first bike person might ring a bell but the other bikes following don't. Wow I'm supposed to know that. I don't walk at lagoon anymore due to the terrorist bike riders aggressively riding and bumping their way through. There are no restrictions on bike riders speeding through and on top of everyone else. There is no speed limit and they can have dogs on leash. Or off leash because they know noone will catch them. Make it a walking only track.
37	Please make signs to ensure cyclists have warning bell on bike for approach. If walking with a friend you need to know to step behind in time. Like they do in Canberra Thank you
38	Please provide a separated cycleway instead of a shared path. Pedestrians walk side-by-side (not single file) and there is too large a speed difference between cyclists and pedestrians.
39	Really good idea. Now just need a decent shared path through Dee Why town centre, as opposed to around it.
40	Shared paths are a great idea to protect cyclists from general road traffic. However, Australian cyclists, unlike European urban cyclists, don't use their bell to warn pedestrians and travel too fast. I would endorse the shared path proposal, but ask that it include a communications plan for cyclists to make the shared paths safer for pedestrians. Perhaps a combination of signs on the routes as well as social media? The problem is that most bike riders have not installed a bell anyway!!
41	Shared paths are GREAT but they do have a major problem. They are not always safe for pedestrians. When cyclist zoom along at speeds that equal car travel, dinging or not dinging bells, it does NOT feel safe for pedestrians. Issues to be addressed for shared paths include: [*] what is the speed limit? [*] what's allowed on the shared path - people walking, prams, bicycles, mobility scooters, people running, skates, scooters, dogs, ELECTRIC bicycles, etc. All of them ????? [*] conduct. For such mixed traffic, 'rules of the road' are needed. Think it's heading for a nanny state? There was a time when foot paths were for pedestrians but nowadays 'foot' paths have become just 'paths' and anything not powered on four wheels goes.

No.	Comments
42	Shared paths are not fit for purpose with the increase in cyclists and the rise of ebikes who travel at a consistent 25kph. It's a bad compromise solution that doesn't suit either pedestrians or cyclists and at best creates antagonism between users and at worst accidents. There's already footpaths on this route, so this project must be changed to a dedicated segregated cycle path so the beaches can start to have a proper cycle active transport network.
43	Sounds good and much needed. Thank you! I use the shared path from the mall to Curl Curl regularly but have only attempted to reach DY via Griffin Street which isn't too busy but very steep! A clear and more level route would really help increase cycling in the area. Please consider making the cycle path continuous across side streets - ie reduce the radius of the kerb to reduce the width of the entrance to the side street and raise the street to pavement level - cars will be forced to slow down and give way to cyclists. I would like to see similar interventions along the Pittwater Road shared path in North Manly. At the moment, the route is not safe for unaccompanied children due to the numerous side street crossings.
44	Sounds great. I am a cyclist and a fan of anything that encourages people to get out of their cars and cycle for local transport. The route needs to be away from traffic and car fumes or it defeats all the health benefits of cycling.
45	Sounds like a good idea
46	Thank you for your great ideas. One part of the path I think could be dangerous. People won't ride down Mitchell. They will stay on Pittwater road because it's so much less hassle and a direct way to reach the b line. Please keep the path on this main road. Plus Mitchell is dangerous... cars on both sides, so many roundabouts, so many driveways and big vans. So dangerous. I would not ride there. Thank you
47	<p>Thank you. North - south bike paths along Pittwater road will be much improved with this latest development. Thank you.</p> <p>My suggestions for next stage:</p> <p>East - west bike paths along Warringah road linking to Pittwater Road and existing Harbord Road to Manly paths And Bike paths linking south - east from Warringah road down Beacon Hill road to Warringah mall and Manly would be my priority. See green lines in submission attachment 1 below. Thank you</p> 
48	<p>Thanks for the opportunity to comment.</p> <p>There is little space nor time so its a matter of being rather direct which can seem undiplomatic. Firstly - share paths don't work effectively. There is too much conflict with bicycles travelling at 30km/h and walkers travelling between 2-5km/h. This is well proven.</p> <p>Ideally Council should see professional advice as to bike paths from consultants with decades of experience in the Netherlands and Denmark. The current Australia paradigm is amateurish at best. Consider, for example, the grade from Dee Why Hill down into Mitchell street.</p> <p>Speeds of up to 60km/h are easily achieved by a bicycle on that segment. I know from personal experience riding the road as its dangerous not to stay at the same speed as general traffic.</p> <p>This means most cyclist (as at the spit Mosman) will NOT use the shared path and remain on the road as pram pushers with kids, elderly walkers etc are a significant hazard to cyclists on shared paths. Cyclists are also a hazard to such users at speeds over approx 15km/h.</p> <p>There is already significant conflict on the not urban Narrabeen lake cycle path. Every weekend</p>

No.	Comments
	<p>there are many near misses between slow users and cycles. It DOES NOT WORK.</p> <p>The design of the infrastructure in terms of accessibility is good as mitchel road would be a good cycle arterial except its underused given the round about are VERY DANGEROUS to cycles.</p> <p>This means that even with a shared path and the required pedestrian style crossings at the round-about it will NOT WORK as cyclists will either step off the shared path or find other ways to maintain momentum and be noticed visually by motorists at the roundabouts.</p> <p>There is a safe design solution for separated cycleways at roundabouts but it does not work as a shared pedestrian system.</p> <p>Furthermore the shared arterials entering the proposed network - the yellow existing - are already poor and under utilised as they are an afterthought system with sharp corners, power poles in dangerous positions etc and if a time based observation is undertaken it can be demonstrated that cyclists will predominantly remain on the roads in preference to the afterthought design as it is safer.</p> <p>As per previous submissions to the bicycle plan I strongly advise NBC to obtain PROFESSIONAL ADVICE in respect of the proposed cycling expansion as I can see my rates being WASTED on poor design that actually does not address demand, does not increase cycling and causes more conflict and hate against cyclists from both vehicle drivers and pedestrians.</p> <p>Only City of Sydney council are delivering solutions appropriately in NSW. Consider their methodology and approach and seek to replicate it as a minimum.</p> <p>1) Provide separated cycleways - purpose built</p> <p>2) Alleviate existing danger spots - ie: Wakehurst Parkway bridged with NO VERGES and dangerous drainage Channels on the existing verges (understand this is NSW Gov but Council can lobby )</p> <p>3) Provide pedestrian friendly open spaces and level pathways without trip hazards and separate pedestrians from roads and cycling.</p> <p>Treat this as a major infrastructure proposal, engage professionals, and please stop wasting my rates on amateur 'solutions'.</p>
49	<p>The existing shared path from Manly should connect up in front of the Bus Depot to the proposed Pittwater Road path. Without this connection, it is a very tenuous link from south to north.</p> <p>Thankyou.</p>
50	<p>The path needs to start further south-west so it connects to the "Connection to Manly Strategic Centre". This will allow cyclists to access the "grade crossing to warringah mall" which was closed off with the introduction of the barriers (this was a shame, making cycling actually more difficult in favour of car traffic!)</p>
51	<p>The proposal assumes that all people not driving are not quickly trying to get from A to B. Cyclists who are trying to move fast are not compatible with walkers, prams, wheelchairs and mobility scooters!</p> <p>The proposed route removes the downhill benefit of Pittwater Road to cyclists and replaces it with dangerous roundabouts and more stops at junctions. This will result in the track not being used by bikes.</p> <p>I expect that few cyclists would use this proposal and even fewer 'walkers, prams, wheelchairs and mobility scooters' as it increase the distance walked, is not scenic and the waiting times at roads will be significant.</p>
52	<p>The proposed shared path link is a sensible and much needed connection to improve the safety of cycling in Brookvale.</p> <p>I'd like to suggest that the path along Winbourne Road between Harbord Road and Mitchell Road also be completed in this first initial stage. This is a common route due to its link to Manly Selective School and it should be connected into the existing network ASAP, and not left for a later date.</p> <p>The proposed other future paths are also much needed and are good suggestions.</p> <p>A future shared path should also run along the North side of Headland Road, between St Lukes Grammar School at Quirk Street, and Harbord Road. This would connect the existing network to St Lukes, and could be extended along Headland Road to the existing bike path at Griffin Road in the future.</p>
53	<p>These proposed routes are good, they'll help us to ride to Warringah Mall and connect through to Dee Why. I hope they will be more than just taking part of the footpath though, as we see with the</p>

No.	Comments
	current shared paths. It's time to allocate space on the roadway for bikes, and to protect riders from cars with physical protection.
54	This is a fantastic idea. Well done council.
55	This is a good plan. There is a real need to extend cycleways in our community.
56	This is an excellent initiative with so many people keen to cycle and walk rather than taking public transport
57	This is such a great initiative which will be great for the area. Given that there will be an increase in foot and bicycle traffic through the Brookvale industrial area the council might also want to consider implementing speed reducing devices such as speed bumps or road obstacles. Some of the streets are currently used for drag racing by motor enthusiasts especially around the Winborne/Carter road end of the industrial area with complete disregard to the safety of other road users. More families with children are frequenting the area for after school and weekend activities and speeding cars are a great concern.
58	This would be great to link major areas of the area. This is very important for riding with younger children. The footpaths between the mall are already being used for this purpose.
59	Too much emphasis has been given to bike Riders who think they own the roads. They don't even pay like the car owners. Very difficult when coming across a group who couldn't care less about other road users.
60	What a great proposal. It will encourage more people to walk or ride their bikes.
61	While I comment the council to improve the facilities and conditions for cyclists around the Northern Beaches, I do have general concerns about the approach taken. Pushing cyclists onto the footpath is in many cases less than helpful to encourage more cycling and to reduce motorised traffic. In fact most of the proposals in this plan and others seem to have the main objective not to inconvenience motorists, never mind the poor pedestrians having to share their path with cyclists, or the cyclist who have to move at walking speed to avoid being sued by pedestrians walking around staring at their phones. A prime example on where the MTA's and council's priorities are can be found at the removed pedestrian crossing across Pittwater road near the Warringah Mall bus stop. Access to the mall is no next to impossible for cyclists approaching from the East and South East. The tiny lift up to the pedestrian bridge can barely cope with normal pedestrian, stroller and wheelchair traffic, let alone cater for the large number cyclists who used to cross at the light controlled pedestrian crossing. There are numerous roundabouts along the proposed route with quite heavy traffic at times. These are difficult and dangerous to negotiate by cyclists moving along the adjacent footpath. Any experience commuter or sport cyclist is much better off staying on the road and flow with the traffic. Children and family outings may be better served by shared paths, but I can't see many of those using this particular route. If the council is prepared and committed to bring any real improvement for cyclists, it should take example of any of the major cities in Europe. They took a much more radical approach to reduce motorised traffic and speed and made a real effort to give cycling priority in terms of space and safety.
62	Why not join the existing Brookvale ( <i>sic</i> ) Oval path to the new one while you are at it? Seems like a small addition.

## Appendix B - Submissions received via email or in writing

Good Afternoon,

I am concerned in regard to your ref 2020/488184, I can not find this on your website for the shared pathway.

Mitchell Road is one of the busiest roads in Brookvale are you suggesting that it will no longer have parking in front of the business's and this will now be a bike lane?

Do you realise how busy Brookvale is to have a bike path?

We have big trucks turning up all day delivery goods??

This will be total mayhem with a lot of road rage I assume?????



[REDACTED]

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Att. Phillip Grey .

I have owned and run businesses in the Brookvale area since 1971 .

I have recently sold my business at [REDACTED] Chard Rd . [REDACTED]

Brookvale is a mess with traffic and people in the main industrial areas . [ with respect ]

Your plan to redirect Orchard /Mitchell Rds to Cycle will cause more Chaos

The existing [ marked yellow on your plan ] is the correct way to go .It has obvious space on all roads , away from the industries and traffic .

It also links with the other East cycle paths from the east side , Curl Curl etc .

Make it safe , stay on the yellow path plan .

[REDACTED] Wattle Rd Brookvale 2100 .

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### New Shared Paths Proposals

1. Brookvale. Fully supported, as they expand the bike network and incorporate key connections.
2. Queenscliff. Also fully supported for the same reasons.

(Email contained additional text in relation to Queenscliff Shared Path Project)

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General Manager  
Ray Brownlee  
Northern Beaches Council  
P.O. Box 82  
Manly NSW 1655

8 September 2020

Dear Mr Brownlee,

**Re: Proposed Path Upgrades – Brookvale to Dee Why and Freshwater to Queenscliff.**

Thank you for the opportunity to comment on the Proposed Path Upgrades– from Brookvale to Dee Why and Freshwater to Queenscliff. Northern Sydney Local Health District (NSLHD) Health Promotion is committed to ensuring that the built environment has a net-positive impact on the health and well-being of individuals and the wider community on the Northern Beaches. In this context, we commend Northern Beaches Council's commitment to increase safer cycling and walking as healthier alternatives to using a car. These strategies are supported by evidence-based guidelines as important contributors to the health of the population<sup>1</sup>.

**Proposed Path Upgrade – Brookvale to Dee Why**

Health Promotion commends Council's proposal to extend the shared path network from Warringah Mall and Brookvale B-Line to Dee Why. The paths would be widened to 2.5m to allow for usage by cyclists and pedestrians. Whilst the 'gold standard' for paths is to separate cyclists from pedestrians, it is appreciated that retrofitting these is not always practicable due to road space constraints and decreased car parking and reduced access to shop fronts that this may cause.

**Proposed Path Upgrade –Freshwater to Queenscliff**

Health Promotion commends Council's proposal to extend the shared path network to connect Manly and Queenscliff to Freshwater Village. However, the proposed route raises safety concerns. It has five street crossings, three of which are considered to be dangerous. Cars travel at speed along Crown and Queenscliff Roads, making them dangerous to cross, despite two pedestrian refuges on Queenscliff Road. In addition, there would be no safe method of crossing the road at the junction of Lower Greycliffe St and (lower) Bridge Rd, which is a sharp, steep bend as it approaches Queenscliff Bridge.

Furthermore this is a long-winded route between Queenscliff and Freshwater Village. The vast majority of people take the direct route – the steps up from Lower Greycliffe St to Pavilion St and down from (upper) Bridge Rd to Undercliff Rd and this is unlikely to change by upgrading the path.

**Recommendation:**

NSLHD Health Promotion recommends Council consider three alternate routes, beginning with the Health Promotion's most preferred:

- 1) To capture the vast majority of pedestrians and cyclists who traverse this route, redirect the proposed shared path, so that it runs down Lawrence and Albert Sts and turns right into Moore Rd. Moore Rd is a very wide road that could cope well with a 2.5m shared path or better still, a separated cycleway. From Moore Rd the path would link to the south-eastern end of Undercliff Rd.

<sup>1</sup> Kent, J., Thompson, S.M., and Jalaludin, B., Healthy Built Environments: A review of the literature, Sydney: Healthy Built Environments Program, City Futures Research Centre, UNSW

At this location Council would construct a switch-back to (upper) Bridge Rd, to serve the same equity and access purposes as the switch-back Council is designing on the south side of Queenscliff Headland or tunnel through from Undercliff Rd to Lower Greycliffe Rd, thereby avoiding constructing two sets of switch-backs. This route would also avoid any road crossings and would therefore be much safer than Council's proposed plan.

Health Promotion appreciates that a tunnel may be prohibitively expensive, however construction of two switchbacks is also an expensive undertaking, so Health Promotion recommends Council weigh up the costs and benefits prior to proceeding.

- 2) Redirect the shared path so that it runs up Dowling St, eastward along the northern side of Crown Rd, then right into (upper) Bridge Rd, to link to the switchback being designed by Council from (upper) Bridge Rd to Lower Greycliffe St. This route would still have five road crossings, however none of these are in dangerous locations and it would be more direct than the proposed route.
- 3) A minimal change to Council's exhibited route would be to continue the shared path along the northern side of Queenscliff Rd into Pavilion St, turning right into (upper) Bridge Rd and linking with the switch-back being designed by Council from (upper) Bridge Rd to Lower Greycliffe St. This would remove the dangerous road crossings at Queenscliff Rd and the corner where Lower Greycliffe St meets (lower) Bridge Rd.

Building shared paths is an expensive undertaking and Health Promotion believes that it is highly unlikely that Council will be able to afford to build another shared path between Queenscliff and Freshwater for a very long time, so it is important that the safest, most beneficial and practical route is chosen for this path.

Thank you for the opportunity to comment on these two important shared paths which will encourage active transport for not only Brookvale, Dee Why, Freshwater and Queenscliff residents, but for all Sydneysiders and tourists to the Northern Beaches to enjoy.

Yours sincerely,



David Morrissey

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Brookvale Community Health Centre,  
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**ITEM 4.12** **PEITA CRESCENT, MONA VALE - NO STOPPING  
RESTRICTIONS****REPORTING OFFICER** **ENGINEER - TRAFFIC****TRIM FILE REF** **2022/157567****ATTACHMENTS** **1 Peita Crescent, Mona Vale - Plan**  
**2 Table of Consultation****GEOCODES: -33.671820, 151.294433****REPORT****BACKGROUND**

Council has received concerns from local residents regarding reduced visibility in Peita Crescent when vehicles park on the approach to the intersection with Parkland Road, Mona Vale.

**LOCATION**

- Peita Crescent is a local road with a 50km/h speed limit.
- The road width is approximately 7.3m wide between kerbs and there are no constructed footpaths.
- Peita Crescent forms a 'Y' intersection with Parkland Road. There is no existing signage or linemarking at the intersection where the 'Give Way' priorities apply.
- There are no signposted parking restrictions in Peita Crescent, however the statutory 'No Stopping' restrictions applies 10m from the intersections with Parkland Road.

**ISSUES**

- Local residents have raised safety concerns when entering and exiting Peita Crescent near the intersection with Parkland Road.
- Peita Crescent intersects Parkland Road on a curve at the bottom of the hill.
- Traffic sight distances are restricted when vehicles park on the inside of the curve, creating a potential traffic hazard to road users and increasing the risk of head-on collisions.
- Vehicles parked on both sides of the road also narrow the overall road width on the approach to the intersection.

**PROPOSAL**

- Council has undertaken a review of the location and issues and proposes to extend the statutory 10m 'No Stopping' restrictions from the intersection, for a distance of 25m along the inside of the curve in Peita Crescent. It is also proposed that the statutory 10m 'No Stopping' restrictions from the intersection with Parkland Road be signposted. A plan of the proposal is shown in Attachment 1 - Peita Crescent, Mona Vale - Plan.
- The proposal will result in the loss of four on-street parking spaces. It is considered reasonable that the request be granted to improve sight distances and safety on the approach to the intersection.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities.
- The proposal does not affect the pedestrian facilities or impacts on walking paths.

**CONSULTATION**

Consultation letters have been distributed to 14 properties within the immediate vicinity of the location providing notification of the proposed changes. A summary of the responses are noted in Attachment 2 – Table of Consultation.

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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the following proposals in Peita Crescent, Mona Vale:

- A. Installation of signage for the statutory 10m 'No Stopping' restrictions from the intersection with Parkland Road.
- B. Extension of 'No Stopping' restrictions (25m) along the inside of the curve; from the intersection, to opposite the driveway of No.22 Peita Crescent, Mona Vale.



## Table of Consultation

<b>Address</b>	Peita Crescent, Mona Vale
<b>Proposal</b>	No Stopping restrictions

<b>Properties Consulted</b>	26
<b>Responses Received</b>	4
<b>Support</b>	4
<b>Do Not Support</b>	0

<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
No Stopping restrictions	<p>- SUPPORT. From the drawings, the 'no stopping' will only be on one side of Pieta Crescent – this is better than the existing situation but 'no stopping' on both sides to the same level would be even better</p> <p>- We support the proposal. Parking has become quite a problem on our street over the past few years, with people parking in dangerous positions. We welcome the changes and request that further changes are made being:</p> <ul style="list-style-type: none"> <li>• Make the small section of kerb between 20 and 22 Pieta Cr a no stopping zone also, it is incredibly difficult for us and our neighbors to maneuver our vehicles when a car is parked in this spot. If this doesn't happen, people will park here creating issues once changes come into effect.</li> <li>• Ban the parking of trucks, commercial vehicles and buses on the hill – this often creates a tight zone to drive through. In addition it creates visibility issues.</li> </ul>	<p>- Parking restrictions are only proposed in the critical areas along the inside of the curve where both the pavement width and sight distances are most restricted due to the road geometry.</p> <p>- The suggested location is situated on a straight section of the road away from the curve, and there are no proposals to further remove parking where it is not required.</p> <p>- The Road Rules do not allow long and heavy vehicles (exceeding 7.5m in length or with a Gross Vehicle Mass (GVM) of 4.5 tonnes) to stop on a length of road for longer than 1 hour in a built up area, and should be reported to Council's Rangers for investigation.</p>

<b>ITEM 4.13</b>	<b>BINBURRA AVENUE, AVALON BEACH - NO STOPPING RESTRICTIONS</b>
<b>REPORTING OFFICER</b>	<b>ENGINEER - TRAFFIC</b>
<b>TRIM FILE REF</b>	<b>2022/157583</b>
<b>ATTACHMENTS</b>	<b>1 Binburra Avenue, Avalon Beach - Plan 2 Table of Consultation</b>

**GEOCODES: -33.623401, 151.339022**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding reduced sight distances when vehicles park on the inside of the curve in Binburra Avenue, Avalon Beach.

### **LOCATION**

- Binburra Avenue is a local road with a 50km/h speed limit, and a road pavement width of approximately 7.5m between kerbs.
- The section of Binburra Avenue under consideration is unrestricted on both sides of the road.
- Kerb and gutter exists on both sides of the road, however there are no constructed footpaths.

### **ISSUES**

- Binburra Avenue connects North Avalon Road with Milga Road. The section of road under consideration is located on a bend and crest in the road.
- Vehicles parked on both sides of the road narrow the overall road width for through traffic.
- Sight distances are further reduced when vehicles park on the inside of the curve, creating a potential traffic hazard to road users and increasing the risk of head-on collisions.
- Binburra Avenue is predominantly used by local residents, however, demand for on-street parking exists due to overflow of parking from residents, as well as visitors of these households.

### **PROPOSAL**

- Council has undertaken a review of the location and issues and consulted on a proposal to install 'No Stopping' restrictions from opposite the driveway of property No.66 to a location 12m north of the driveway to No.43 Binburra Avenue, to prevent parking on the inside of the curve.
- The proposal acts to prevent parking in the most critical areas along the inside of the curve, where traffic sight distances are restricted by the curvature in the road, and where parking is not suitable on both sides due to the narrow road width.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- Improves safety for all road users including people cycling along Binburra Avenue by providing a wider road and improved sightlines along the bend for through traffic.
- The proposal does not affect any pedestrian facilities or impacts on walking paths.

**CONSULTATION**

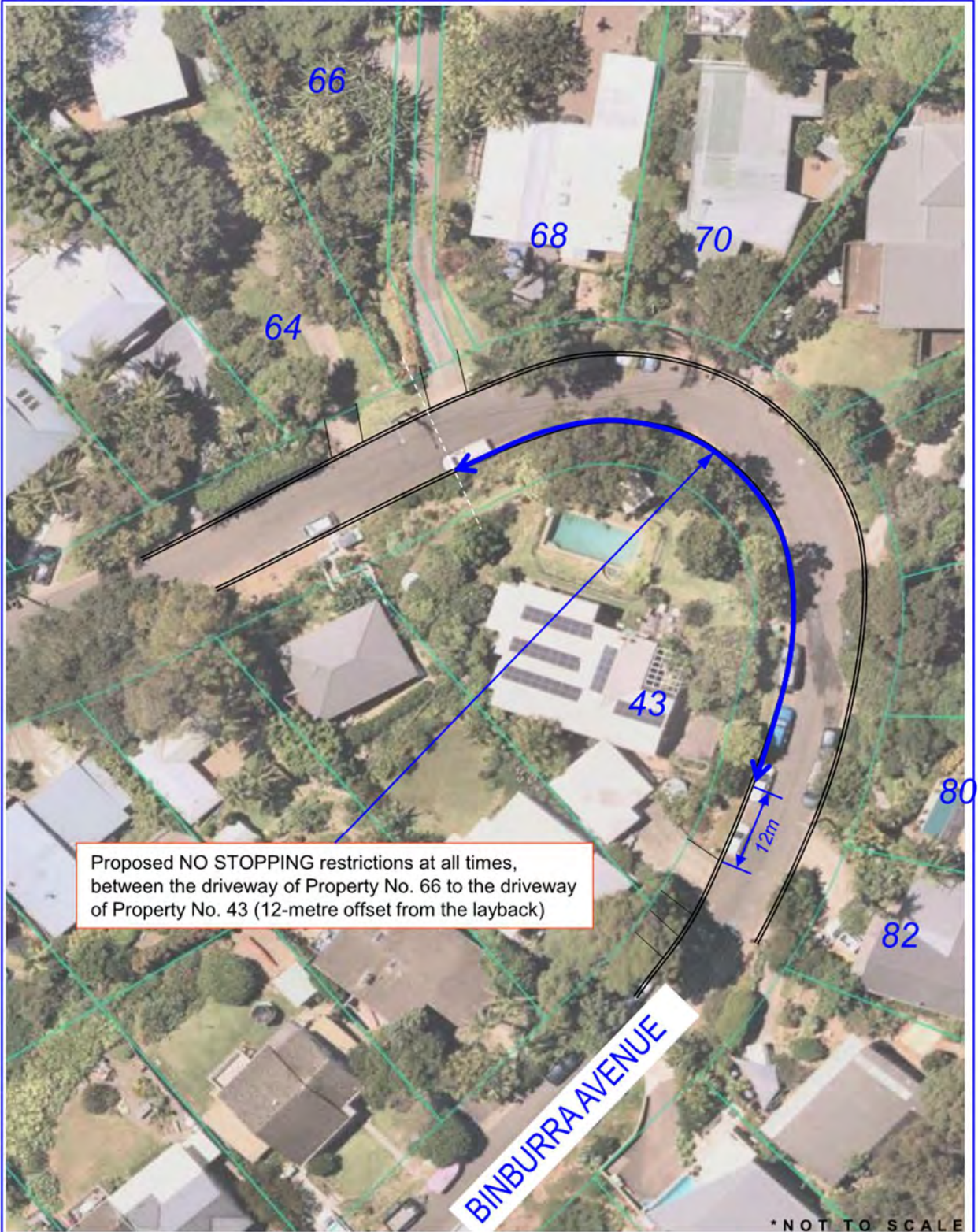
- Consultation letters have been distributed to 26 properties within the immediate vicinity of the location providing notification of the proposed changes. A summary of the responses is noted in Attachment 2 - Table of Consultation.
- Council has noted some of the concerns regarding the loss of on-street parking and has made some minor changes where possible to reduce the length of the restrictions. The amended proposal will enable up to 3 vehicles to park between the driveway of No.43 and the start of the new restrictions, to minimise the impact. The updated plan of the proposal is shown in Attachment 1 - Binburra Avenue, Avalon Beach - Plan.

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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the:

- A. Installation of 'No Stopping' restrictions from opposite the driveway of property No.66, to a location 16m north of the driveway to No.43 Binburra Avenue, Avalon Beach.



	PROPOSAL						
	Binburra Avenue, Avalon Beach No Stopping Restrictions						
	DRAWN	BD	APPROVED				
	LAYOUT	1 OF 1	REVISION NO.	A	DATE	09/02/2022	

## Table of Consultation

<b>Address</b>	Binburra Avenue, Avalon Beach
<b>Proposal</b>	No Stopping restrictions

<b>Properties Consulted</b>	26
<b>Responses Received</b>	6
<b>Support</b>	4
<b>Do Not Support</b>	2

Issue	Resident Comment	Council Response
No Stopping restrictions	<p>- I am totally against this proposal. I can only imagine the complaint has come from someone new to the area who isn't used to the narrow, steep and congested roads in our area. By removing the parked cars you do not allow anymore vision of the blind corner as it is too steep and drops away sharply to the left. The parked cars actually slow the on coming traffic and ensue people take care negotiating the corner. Most homes in our street have more than 2 cars so there is little free parking. By restricting the parking on the western side of the street you just create a huge problem further down the road where we will all be fighting for spots.</p> <p>- I proposed this same motion to council several years ago but it was rejected immediately. Yes it is dangerous for sure going into the corner mentioned however starting the no stop zone from 66 upwards does not solve the problem but pushes the problem further down the hill affecting residents.</p>	<p>- The primary function of roads is to facilitate a safe traffic flow, with public on street parking provided only where appropriate. The proposal improves visibility through the curve and provides additional road width for oncoming traffic.</p> <p>Council has however noted concerns regarding the loss of on-street parking and has reduced the length of the 'No Stopping' slightly to enable up to 3 vehicles to park between the driveway of No.43 and the start of the new restrictions to minimise the impact.</p> <p>- Parking restrictions are only proposed in the most critical areas along the inside of the curve and where the pavement width and sight distances are both restricted due to the road geometry. Relocation of parking to the straight section of road is safer than permitting the current parking on the bend. □</p>
Parking for residents	<p>- We would strongly suggest the council reconsider the proposal by considering alternative options, such as:</p> <p>a) restricting street parking to two cars per household (to owners only) and/or</p> <p>b) decreasing the size of the area of the 'No Stopping' restrictions from the opposite driveway of the property No. 66 to the alternate entry point of No. 43 (opposite the driveway of No. 78)</p> <p>- I am in support of the no stopping restrictions. As a resident of Binburra Avenue for 8 years, we do not use that side of the road due to the safety concerns i have. I understand parking is in high demand, however, we have properties that have either unusable or no driveways - they have opted for gardens instead. Our driveways are steep and can be challenging to use, however, it is possible for those of us who have them to use them instead of parking on the street.</p>	<p>- Council would not support restricting on-street parking to 2 cars per household, as this could mean that some vehicles would be forced to park in Milga Street which is 250m further away. Instead the proposal would prevent parking in the most unsafe locations and encourage that residents better utilise their existing off-street parking according to their needs.</p> <p>- Parking for private vehicles remains the responsibility of the property owners, while on-street parking is public parking which is available for everyone. Residents with multiple vehicle who are unable to provide for all of their off-street parking needs or choose to park on the road, must therefore share the available parking spaces with the community.</p>

<b>ITEM 4.14</b>	<b>GRANDVIEW PARADE, MONA VALE - NO PARKING RESTRICTIONS</b>
<b>REPORTING OFFICER</b>	<b>ENGINEER - TRAFFIC</b>
<b>TRIM FILE REF</b>	<b>2022/157813</b>
<b>ATTACHMENTS</b>	<b>1 Grandview Parade, Mona Vale - Plan 2 Table of Consultation</b>

**GEOCODES: -33.674164, 151.317885**

## **REPORT**

### **BACKGROUND**

Council has received a request from URM, Council's waste and recycling contractor; for parking restrictions in the turning circle of Grandview Parade, to ensure waste collection vehicles can access and service the street.

### **LOCATION**

- Grandview Parade is a no through road with a 50km/h speed limit.
- Grandview Parade is approximately 550m in length and provides access to North Mona Vale Headland Reserve.
- The road width is approximately 7m wide between kerbs, and there are no constructed footpaths.
- The turning circle is situated at the south-eastern end of the road with a 15m diameter.
- There are no parking restrictions in Grandview Parade, except for the statutory 10m 'No Stopping' restrictions from the intersection.

### **ISSUES**

- Council has received concerns from URM regarding parking in the turning circle affecting access at the end of Grandview Parade.
- Parallel parking is unrestricted on both sides of the road including the turning circle.
- Larger vehicles including waste collection services and delivery vehicles have difficulty manoeuvring at the end of Grandview Parade when vehicles are parked on the approach to and within the turning circle.

### **PROPOSAL**

Council has undertaken a review of the above location and issues and consulted on a proposal to install 'No Parking' restrictions between the driveways of No.51 and No.66 Grandview Parade. A plan of the proposal is shown in Attachment 1 - Grandview Parade, Mona Vale - Plan.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

Consultation letters have been distributed to 12 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

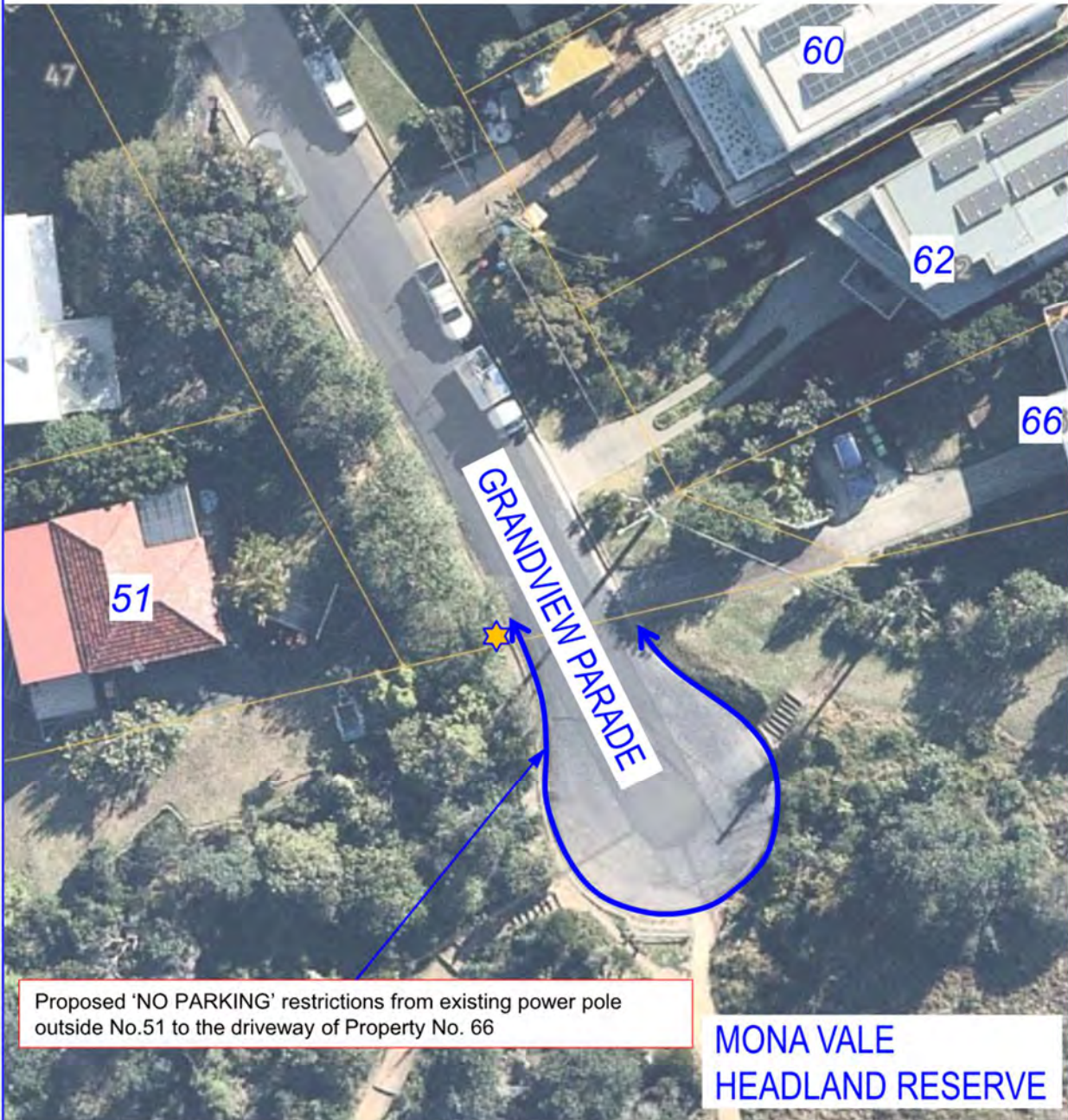
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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the:

- A. Installation of 'NO PARKING' restrictions between the driveways of No.51 and No.66 Grandview Parade, Mona Vale.

←→ Proposed extension of 'NO PARKING' restrictions (55-metres)



Proposed 'NO PARKING' restrictions from existing power pole outside No.51 to the driveway of Property No. 66

MONA VALE  
HEADLAND RESERVE

\*NOT TO SCALE



### PROPOSAL

Grandview Parade, Mona Vale  
No Parking Restrictions

DRAWN BD

APPROVED



LAYOUT 1 OF 1

REVISION NO.

A

DATE

11/01/2022



northern  
beaches  
council

## Table of Consultation

<b>Address</b>	Grandview Parade, Mona Vale
<b>Proposal</b>	No Parking restrictions

<b>Properties Consulted</b>	18
<b>Responses Received</b>	2
<b>Support</b>	1
<b>Do Not Support</b>	1

<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
No Parking restrictions	<p>- We support the no parking area will begin beside the driveway of property No. 62 and continue around to the power pole at No. 51. We do not agree with the alternate proposal shown on the map where the no parking area begins beside the driveway of No. 66. The reason for that is the length of curb between No. 62 &amp; No. 66 is quite short and does not allow enough room for a regular vehicle to park properly without impinging on driveway access – a frequent problem.</p> <p>- We favour the most practical version in the picture, thus retaining the parking space between 62 and 66, and which will not impede the truck access or turning.</p>	<p>- Parking restrictions are only proposed in the most critical areas to enable waste collection vehicles and delivery trucks to use the turning area. It is possible for a smaller vehicle to park between the driveway of No. 62 and 66, and therefore there are no proposals to further remove parking where it is not required. According to the Road Rules it is illegal for vehicles to park on the road blocking a driveway or on the driveway (even your own) outside the property boundary. Any instances of illegal parking should be reported to Council's Rangers for investigation.</p>

<b>ITEM 4.15</b>	<b>INTERSECTION OF KELLYS WAY AND MORGAN ROAD, OXFORD ROAD FALLS - INSTALLATION OF STOP SIGN</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/159591</b>
<b>ATTACHMENTS</b>	<b>1 Plan</b>

**GEOCODES: -33.726540, 151.235962**

## **REPORT**

### **BACKGROUND**

The local community has raised concerns around the traffic conditions at the intersection of Morgan Road, Kellys Way and Oxford Falls Road, with drivers not following the existing Give Way control on Morgan Road. Since the road has been sealed, there has been an increase in near-miss incidents at this location.

### **LOCATION**

- The proposed location of installation of STOP sign has been located on the western side of the intersection of Kellys Way and Morgan Road.
- This section of Kellys Way is a local road carrying low volumes of two-way traffic.
- Kellys Way intersects with Morgan Road to the south and it intersects with Oxford Falls Road to the east.
- The gravel formation of Kellys Way has a nominal width of carriageway of 6.5 metres.
- There is a Give Way sign on the western approach to the intersection of Kellys Way and Morgan Road.
- The land use of the surrounding area is predominantly rural/residential area.

### **ISSUES**

- Vehicles on Kellys Way having priority over the main flow of traffic from Morgan Road to Oxford Falls Road.
- Vehicles on Morgan Road not giving way to traffic coming from Kellys Way – whilst infrequent, the consequence was either an accident or a near-miss incident.

### **PROPOSAL**

Council has undertaken a review of the location and the concerns raised and proposes the installation of a STOP control on the Kellys Way leg of the intersection.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

Consultation letters have been distributed to 6 properties within the immediate vicinity of the location providing notification of the proposed changes.

- No submissions have been received in regard to this proposal.

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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports:

- A. The installation of a STOP control at the intersection of Morgan Road and Kellys Way, on the Kellys Way leg.



<b>ITEM 4.16</b>	<b>OCEAN STREET, NARRABEEN - EXTENSION OF BUS ZONE</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/170849</b>
<b>ATTACHMENTS</b>	<b>1 Plan</b> <b>2 Table of Consultation</b>

**GEOCODES: -33.707378, 151.303452**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding parked vehicles that obstruct access to the Bus Zone near No.183 Ocean Street. This can result in buses being unable to pull in safely, creating a potential hold-up for following traffic and interfering with safe alighting and boarding of buses. There is also a potential for damage to parked vehicles.

### **LOCATION**

Ocean Street is a local road of 12.5m in width, carrying medium volumes of two-way traffic. Ocean Street intersects Octavia Street on its south side and it intersects Tourmaline Street on its northern side. There is a Bus Zone in front of No.181 and No.183 Ocean Street. There is a bus route that services Ocean Street.

### **ISSUES**

- Parked vehicles obstruct access to the Bus Zone near No.183 Ocean Street.
- This results in buses being unable to pull in safely, creating a potential hold-up for following traffic and interfering with safe alighting and boarding of buses.
- There is also a potential for damage to parked vehicles.

### **PROPOSAL**

Council has undertaken a review of the location and issues and proposes the extension of the existing Bus Zone between the northern side of the driveway of No.179 Ocean Street and the northern side of driveway No. 183 Ocean Street.

### **PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

## CONSULTATION

Consultation letters have been distributed to 90 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

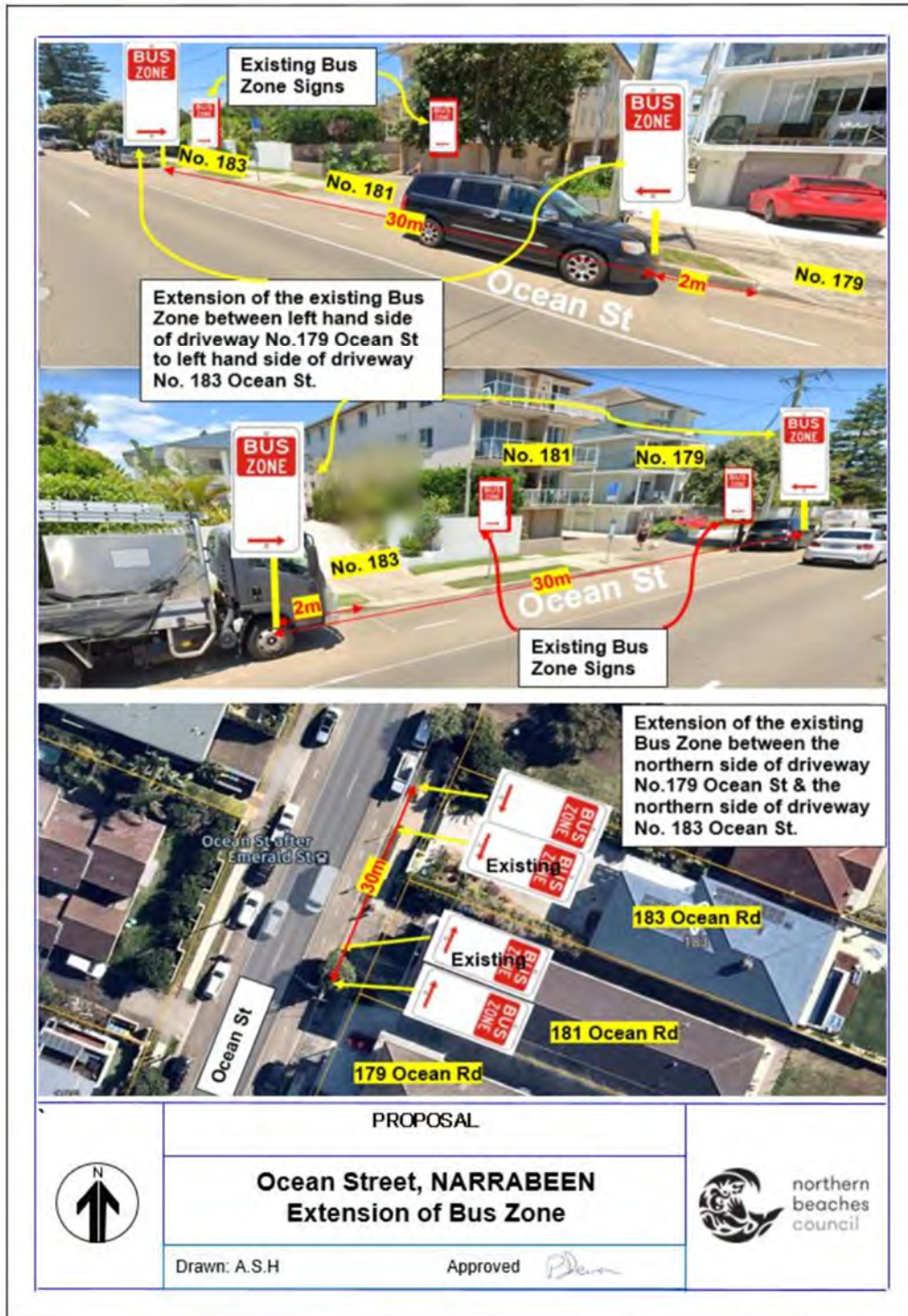
- A total of 7 submissions were received with 5 objections and 2 supporting the proposal.

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## RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee supports:

- A. The extension of the existing Bus Zone between the northern side of the driveway of No.179 Ocean Street and the northern side of driveway No. 183 Ocean Street.



<u><b>Table of Consultation</b></u>		
<b>Address</b>	<u><b>Ocean Street, NARRABEEN</b></u>	
<b>Proposal</b>	<u><b>Extension of Bus Zone</b></u>	
<b>Properties Consulted</b>	<b>90</b>	
<b>Responses Received</b>	<b>7</b>	
<b>Support</b>	<b>2</b>	
<b>Do Not Support</b>	<b>5</b>	
<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
<b>Lack of Parking</b>	To remove two parking spots from Ocean Street is just adding to our parking problems. Parking is already so precious in our Street. Summer time and Nippers on a Sunday is an issue for our residents as tourists come from everywhere and take up our parking spots.	Long term parking of private vehicles must be on private properties as the residents are responsible to utilise garages and existing driveways for off-street parking in case of increased parking demands.

<b>ITEM 4.17</b>	<b>SOUTH CREEK ROAD, CROMER - CONVERSION OF 8P RESTRICTIONS INTO AUTHORISED VEHICLES PARKING</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/179000</b>
<b>ATTACHMENTS</b>	<b>1 Plan</b>

**GEOCODES: -33.738600, 151.283746**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding the School parking facility and requesting a special pick-up and drop-off facility for children who require additional needs. Cromer High school has a large number of students who require additional needs. Some of these children are transported to and from school by private vehicles arranged via the school travel program of the NSW Department of Education. Currently, these vehicles are competing with other vehicles and share the existing pick-up & drop-off area which is not sufficient.

### **LOCATION**

- South Creek Road is a local road about 12.8 m wide with a speed limit of 50km/h.
- South Creek Road is a Bus Route.
- A Pedestrian Marked Crossing is located on South Creek Road near the entrance to Cromer Campus.

### **ISSUES**

- Cromer High School has a large number of students who require additional needs.
- The existing pick-up and drop-off facilities are not sufficient and cause double parking and queuing at school times. Furthermore, the Pedestrian Marked Crossing and Buses cause additional impacts on traffic flow which causes congestion.

### **PROPOSAL**

Council has undertaken a review of the location and issues and proposes the conversion of the existing '8P, 8AM – 6PM, Every Day' restriction into 'No Stopping; 8:00AM – 8:45AM, 2:30PM – 3:15PM, SCHOOL DAYS ONLY, AUTHORISED VEHICLES EXCEPTED, UNRESTRICTED PARKING AT OTHER TIMES.

### **PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

## CONSULTATION

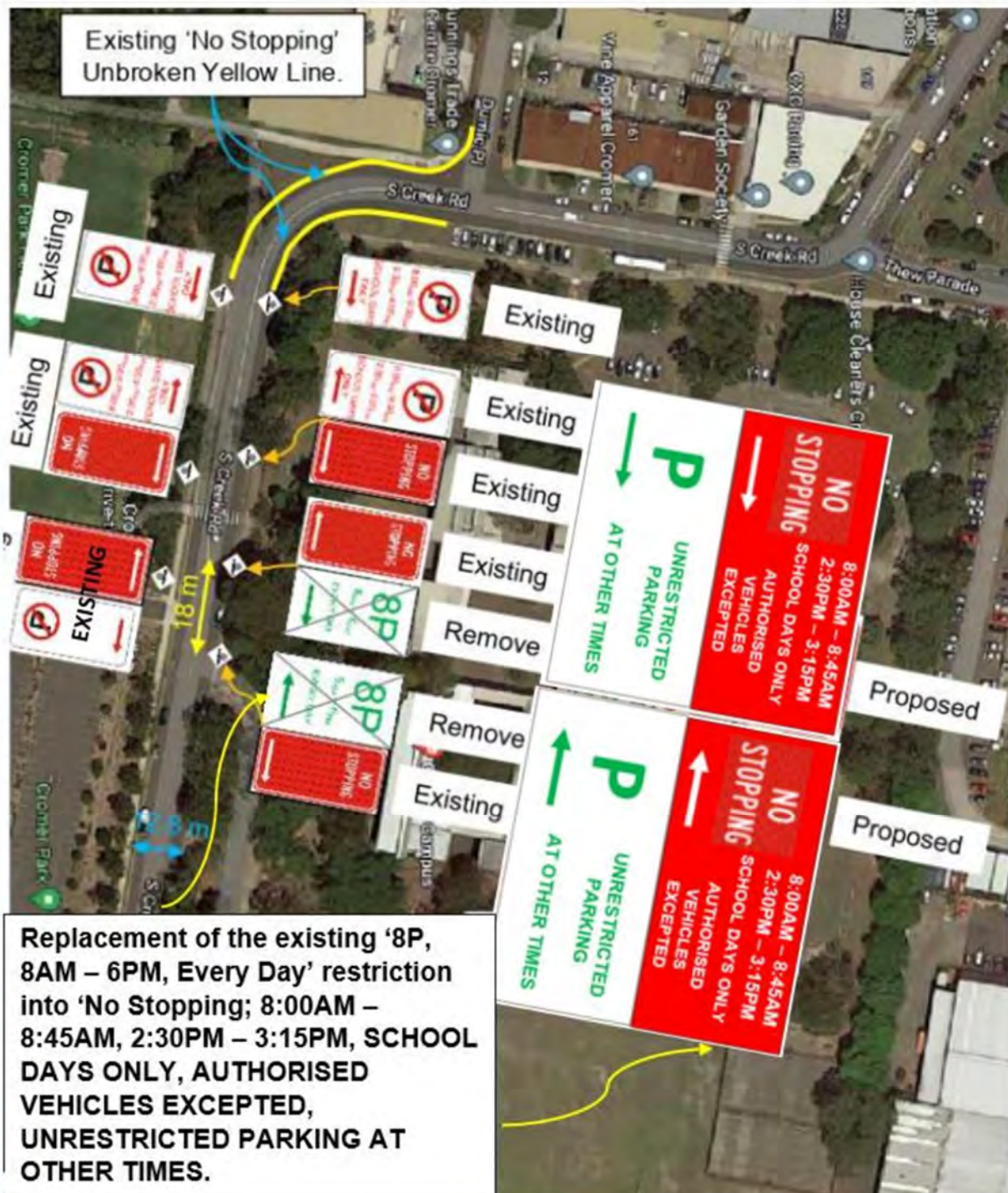
Public consultation is not necessary as the impact on residents due to this proposal is very low and the proposal improves the School Parking facility.

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## RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee supports the:

- A. Conversion of the existing '8P, 8AM – 6PM, Every Day' restriction into 'No Stopping; 8:00AM – 8:45AM, 2:30PM – 3:15PM, SCHOOL DAYS ONLY, AUTHORISED VEHICLES EXCEPTED, UNRESTRICTED PARKING AT OTHER TIMES.



PROPOSAL

**Conversion of 8P Restrictions Into  
AUTHORISED VEHICLES PARKING-  
South Creek Road, CROMER**

Drawn: A.S.H

Approved





northern  
beaches  
council

**ITEM 4.18**                      **IRRAWONG ROAD, NORTH NARRABEEN - INSTALLATION  
OF "NO PARKING, MOTOR VEHICLES EXCEPTED"  
RESTRICTION**

<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/181278</b>
<b>ATTACHMENTS</b>	<b>1 Plan</b> <b>2 Table of Consultation</b>

**GEOCODES: -33.693520, 151.285858****REPORT****BACKGROUND**

Council has received concerns from local residents regarding parking of boats and trailer for long periods of time on the reserve side of Irrawong Road and creating a shortage of parking for the residents and visitors to the area

**LOCATION**

Irrawong Road is a local road of 6m in width, carrying low volumes of two-way traffic. Irrawong Road intersects Epworth Place on its western side and it intersects Inala Place and Indura Road on its east southern side. There is no parking restriction on Irrawong Road. There is no bus route that services Irrawong Road.

**ISSUES**

- Parking of boats and trailer for long periods of time on the reserve side of Irrawong Road create a shortage of parking for the residents and visitors to the area.

**PROPOSAL**

Council has undertaken a review of the location and issues and proposes the installation of "No Parking, Motor Vehicles Excepted" restriction on the northern side of Irrawong Road between Indura Road and Epworth Place.

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

Consultation letters have been distributed to 15 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

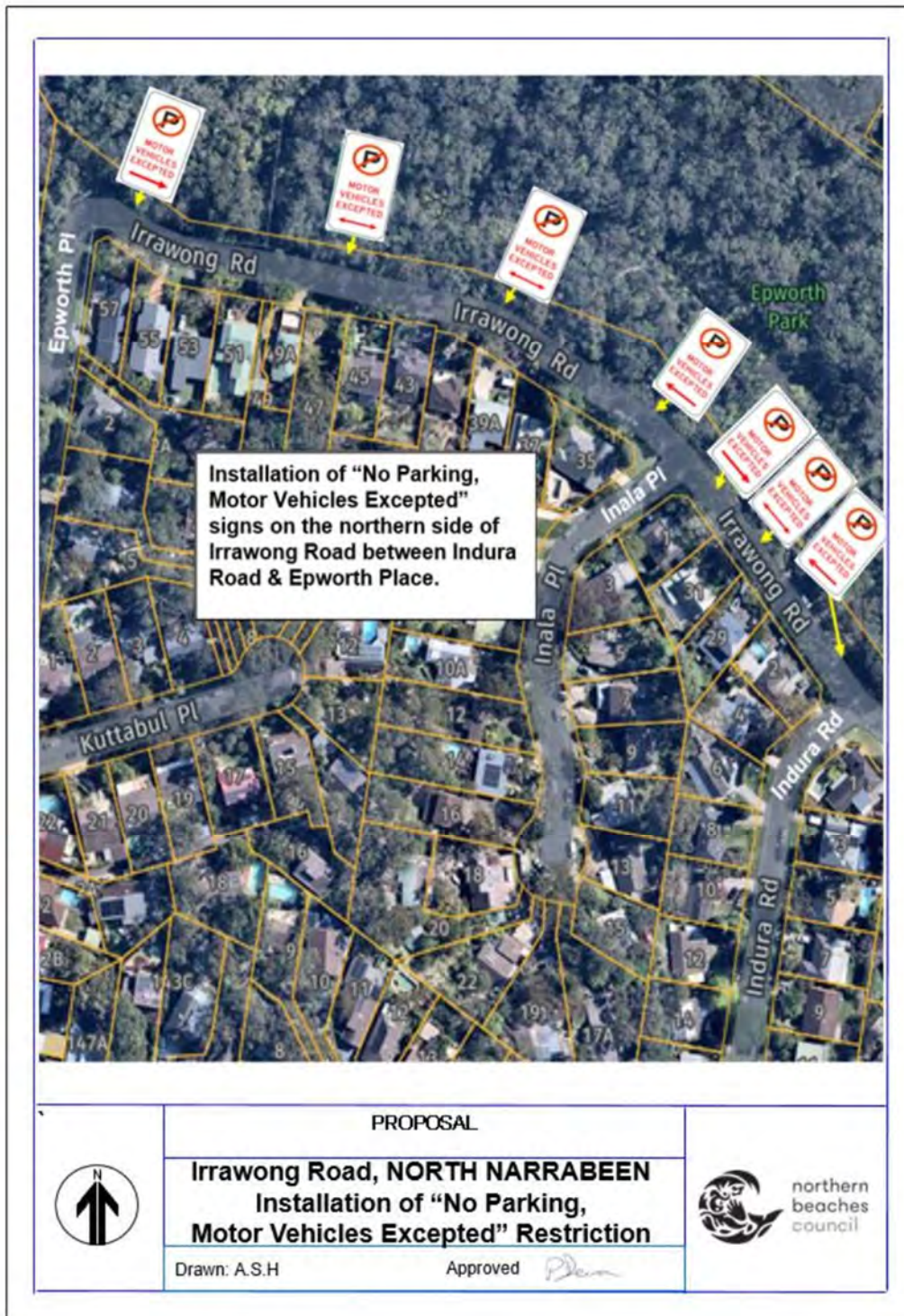
- A total of 6 submissions were received with 4 objections and 2 supporting the proposal.

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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the:

- A. The installation of “No Parking, Motor Vehicles Excepted” restriction on the northern side of Irrawong Road between Indura Road and Epworth Place.



<u><b>Table of Consultation</b></u>		
<b>Address</b>	<b>Irrawong Road, NORTH NARRABEEN</b>	
<b>Proposal</b>	<b><u>Installation of “No Parking, Motor Vehicles Excepted” Restriction</u></b>	
<b>Properties Consulted</b>	<b>15</b>	
<b>Responses Received</b>	<b>8</b>	
<b>Support</b>	<b>2</b>	
<b>Do Not Support</b>	<b>4</b>	
<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
<b>Lack of Parking</b>	There appears to be a boating and trailer dumping ground in Irrawong Rd Narrabeen especially adjacent to Inala place. Is the council able to do anything about this ?	Council proposes the installation of No Parking, Motor Vehicles Excepted” restriction on the northern side of Irrawong Road between Indura Road and Epworth Place in order to resolve the issue by preventing parking of boats and trailers on Irrawong Road for long periods of time.

<b>ITEM 4.19</b>	<b>AMELIA PLACE, NORTH NARRABEEN - INSTALLATION OF NO STOPPING YELLOW LINE</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/184895</b>
<b>ATTACHMENTS</b>	<b>1 Plan</b> <b>2 Table of Consultation</b>

**GEOCODES: -33.699348, 151.288254**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local residents regarding parked vehicles and trailers on both sides of Amelia Place that obstruct access to and from driveways, potentially resulting in damage to parked vehicles. Furthermore, parked vehicles and trailers near the junction of Amelia Place with Powderworks Road restrict carriageway width for passing and also reduces sight distance to approaching traffic

### **LOCATION**

Amelia Place is a local road of 7m in width, carrying medium volumes of two-way traffic. Amelia Place intersects Powderworks Road on its northern side and it ends in a cul-de-sac on its southern side. There is no parking restriction in Amelia Place. There is no bus route that services Amelia Place.

### **ISSUES**

- Parked vehicles and trailers on both sides of Amelia Place obstruct access to and from driveways, potentially resulting in damage to parked vehicles.
- Parked vehicles and trailers near the junction of Amelia Place with Powderworks Road restrict carriageway width for passing and also reduces sight distance to approaching traffic.

### **PROPOSAL**

Council has undertaken a review of the location and issues and proposes the installation of a length of unbroken No Stopping yellow line between the driveway to No.2 Amelia Place and Powderworks Road in addition to the installation of unbroken No Stopping yellow line between the eastern side of driveway No.100 Powderworks Road and the end point of the existing double white line on the western side of Amelia Place.

### **PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have impact on people cycling and does affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

## CONSULTATION

Consultation letters have been distributed to 20 properties within the immediate vicinity of the location providing notification of the proposed changes. The responses are noted in Attachment 2 – Table of Consultation.

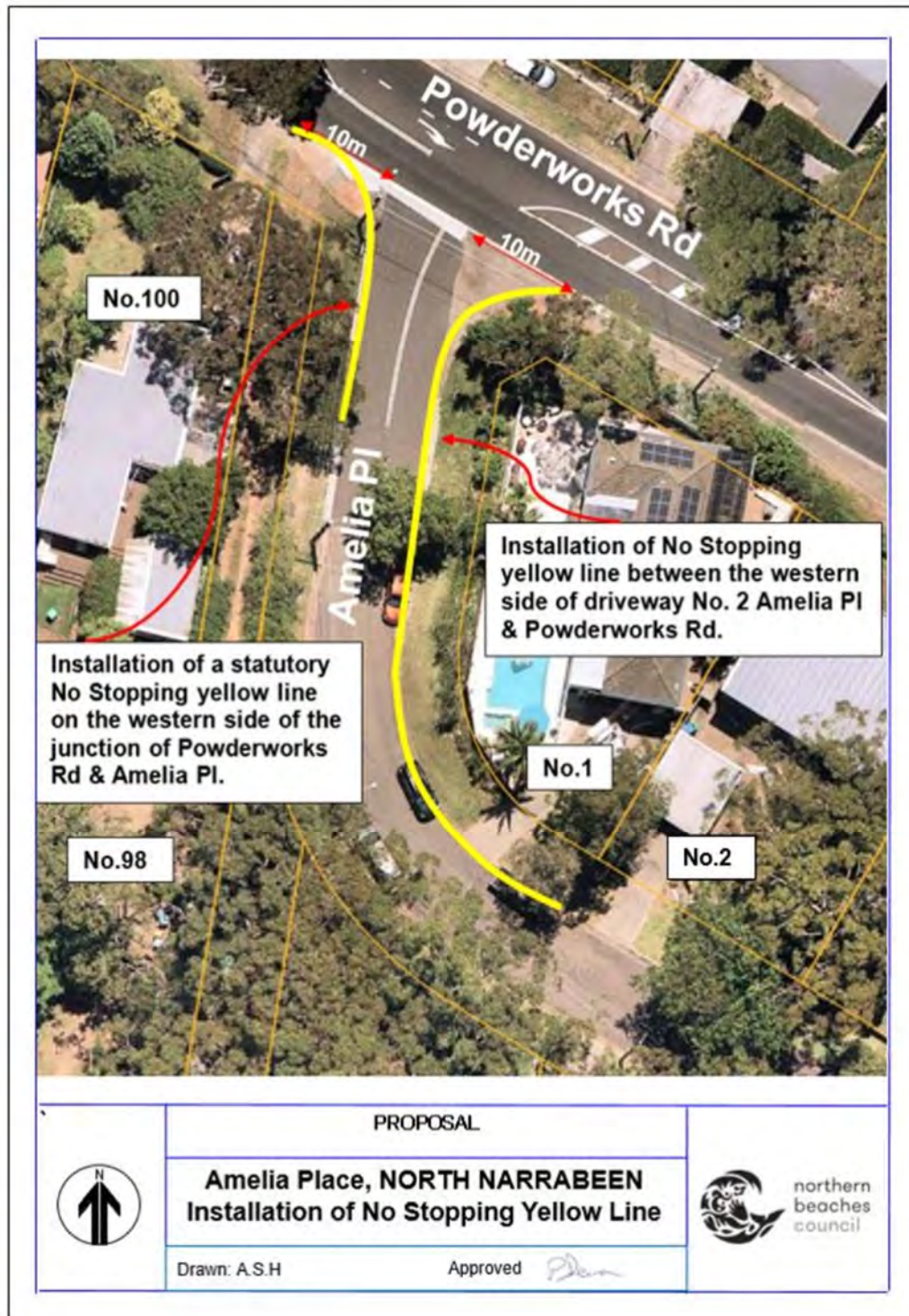
- A total of 6 submissions were received with 1 objection and 5 supporting the proposal.

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## RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee supports:

- A. The installation of a length of unbroken No Stopping yellow line between the driveway to No.2 Amelia Place and Powderworks Road.
- B. The installation of a length of unbroken No Stopping yellow line between the eastern side of driveway No.100 Powderworks Road and the end point of the existing double white line on the western side of Amelia Place.



<u><b>Table of Consultation</b></u>		
<b>Address</b>	<b><u>Amelia Place, NORTH NARRABEEN</u></b>	
<b>Proposal</b>	<b><u>Installation of No Stopping yellow line</u></b>	
<b>Properties Consulted</b>	<b>20</b>	
<b>Responses Received</b>	<b>6</b>	
<b>Support</b>	<b>5</b>	
<b>Do Not Support</b>	<b>1</b>	
<b>Issue</b>	<b>Resident Comment</b>	<b>Council Response</b>
Lack of Parking	Taking away the parking spaces (we are probably talking about 3 spaces total) would be greatly detrimental to the residents living on Powder Works and Amelia. Although it's always nicer to drive along a road with fewer parked cars on the side, nothing really is gained by this proposal except making the street "look" less busy.	Council's proposal will provide a clear carriageway at the proposed location and it will improve the safety issues at the existing bend and around the existing double white lines due to narrow width of the road at these locations. Long term parking of private vehicles must be on private properties as the residents are responsible to utilise garages and existing driveways for off-street parking in case of increased parking demands.

<b>ITEM 4.20</b>	<b>WILLIAM STREET, BROOKVALE - DISABLED PARKING SPACES</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/186398</b>
<b>ATTACHMENTS</b>	<b>1 William Street, Brookvale - Plan</b>

**GEOCODES: -33.76891, 151.27127**

## **REPORT**

### **BACKGROUND**

Council has received concerns from local community regarding the existing kerbside disabled parking spaces outside No.26 William Street, Brookvale.

### **LOCATION**

- William Street, Brookvale/North Manly is a major local collector road, splitting residential and industrial areas.
- The location under consideration of William Street has available road width of approximately 12.3 metres.
- A double centre line is installed at this location
- On-street parking is generally unrestricted on the north side of William Street with the exception of intersections and driveways.
- There are no bus routes that service the section under consideration.
- Footpath is available on both sides of the road.

### **ISSUES**

- The existing parking spaces has a length of 12 metres, which does not provide sufficient length for 2 vehicles to service disable passengers.
- No alternative disabled parking spaces are available within the immediate vicinity on William Street.

### **PROPOSAL**

Council has undertaken a review of the location and issues and proposes to:

- Relocate one of the existing disabled parking signs to increase the length of the parking spaces up to 15.6 m, and
- Install edge lines on the road to delineate the parking spaces of 3.0m (w) \* 15.6m (l).

**PEDESTRIAN AND CYCLING IMPACT STATEMENT**

This proposal will have the following impact(s) on pedestrians and people cycling:

- The proposal will have no impact on people cycling and does not affect any future planned facilities
- The proposal does not affect the pedestrian facilities or impacts on walking paths

**CONSULTATION**

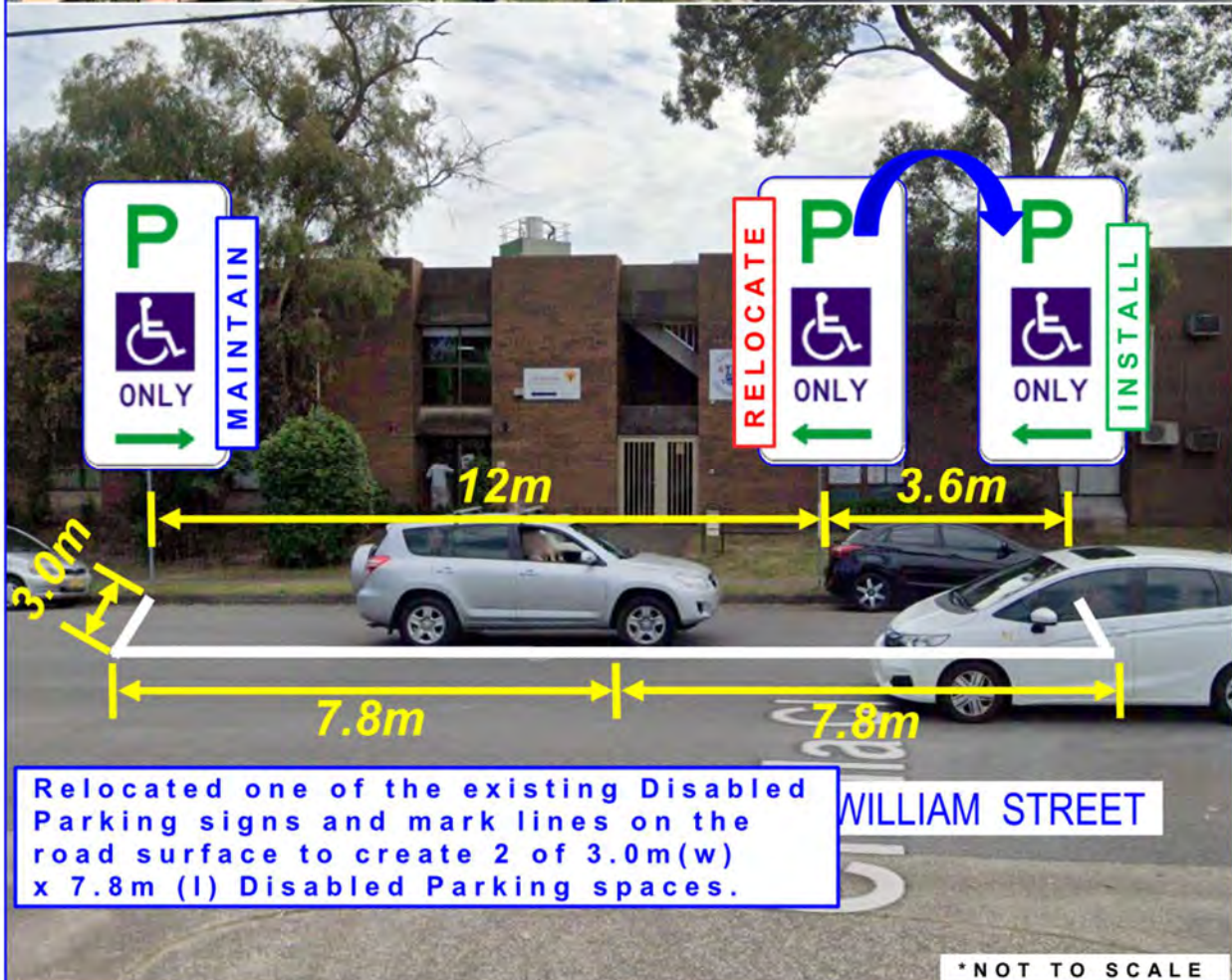
No consultation was conducted as this introduces very limited change on the on-street parking at this location.

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**RECOMMENDATION TO TRAFFIC COMMITTEE**

That the Traffic Committee supports the:

- A. Relocation of one of the existing disabled parking signs, and
- B. Installation of edge lines.



Relocated one of the existing Disabled Parking signs and mark lines on the road surface to create 2 of 3.0m(w) x 7.8m (l) Disabled Parking spaces.

	PROPOSAL					 northern beaches council
	26 William Street, Brookvale Disabled Parking Spaces Adjustments					
	DRAWN	YR	APPROVED 			
	LAYOUT	1 OF 1	REVISION NO.	B	DATE	

## 5.0 MATTERS FOR NOTATION

<b>ITEM 5.1</b>	<b>REQUEST FOR WORKS ZONE</b>
<b>REPORTING OFFICER</b>	<b>TRAFFIC OFFICER</b>
<b>TRIM FILE REF</b>	<b>2022/122171</b>
<b>ATTACHMENTS</b>	<b>NIL</b>

**GEOCODES:** Various

### REPORT

Council has approved the following Works Zone under delegated authority from the Northern Beaches Council Local Traffic Committee to the Traffic Engineer. The Works Zone signs are installed by Council and the applicant is to advise Council of any extension of the Works Zone period prior to the expiry date. The extension is subject to approval by Council's Traffic Engineer.

<b>Applicant</b>	<b>Location</b>	<b>Works Zone Length and Time</b>	<b>Requested Period</b>
Bau Group Pty Ltd 9/10 Prosperity Parade, WARRIEWOOD NSW 2102	10 Molong Street, NORTH CURL CURL NSW 2099	Length: 15 metres Time: 7:00am-5:00pm Mon-Fri 8:00am-1:00pm Saturday	14 March 2022 to 11 April 2022
Mettricon Homes PO Box 7510 NORWEST NSW 2153	12-14 Soldiers Avenue FRESHWATER NSW 2096	Length: 20 metres Time: 7:00am-5:00pm Mon-Fri 8:00am-1:00pm Saturday	4 April 2022 to 3 October 2022

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### RECOMMENDATION TO TRAFFIC COMMITTEE

That the Traffic Committee notes the delegated approval of Works Zones as described above.